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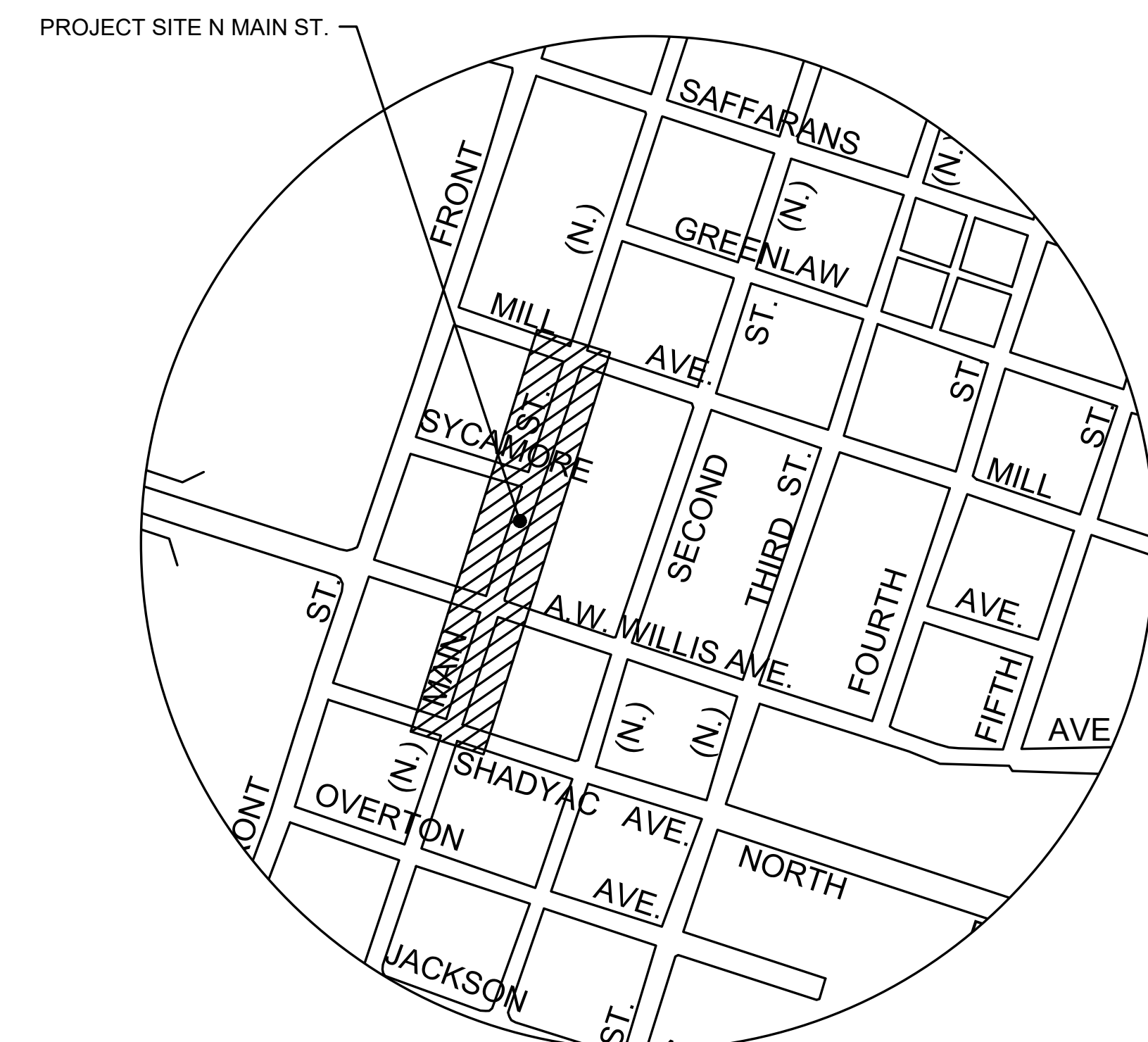
MATA N. MAIN ST. TRACK SLAB REPAIR

FOR

MEMPHIS AREA TRANSIT AUTHORITY
1370 LEVEE ROAD
MEMPHIS, TN 38108



2650 Thousand Oaks BLVD. - Suite 4200
Memphis, Tennessee 38118
Phone: (901) 683-3900 Fax: (901) 683-3990



SCALE: 1"=400'

MARCH 2022

GENERAL NOTES

EXISTING CONDITIONS:

CONDITIONS AND DIMENSIONS OF EXISTING TRACK SLAB IS ASSUMED TO BE AS SHOWN ON THE EXISTING DRAWINGS FROM ORIGINAL CONSTRUCTION.

CONFORMANCE OF EXISTING CONDITIONS TO THE DRAWINGS HAS NOT BEEN VERIFIED BY SSR. ALL CONDITIONS ASSOCIATED WITH ANY AREA SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO PROCEEDING WITH THE WORK IN THAT AREA.

CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF ALL SITE SETBACKS, EASEMENTS AND DIMENSIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

ALL EXISTING (E) CONDITION DOCUMENTATION IS TAKEN FROM (E) CONSTRUCTION DOCUMENTS. (E) CONDITIONS, INCLUDING ELEVATIONS, MAY VARY FROM THOSE PRESENTED IN THESE DOCUMENTS. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS AT THE JOB SITE BEFORE COMMENCING WORK AND SHALL REPORT ANY DISCREPANCY TO THE ENGINEER.

THE CONTRACTOR SHALL CONFORM TO ALL CITY OF MEMPHIS AND MEMPHIS AREA TRANSIT AUTHORITY CODES AND STANDARDS.

THE CONTRACTOR SHALL PROVIDE A SMOOTH TRANSITION BETWEEN EXISTING PAVEMENT AND NEW PAVEMENT. FIELD ADJUSTMENT OF FINAL GRADES MAY BE NECESSARY.

ANY SIGNAGE REMOVED DURING CONSTRUCTION SHALL BE REINSTALLED AFTER CONSTRUCTION IS COMPLETE. SIGN INSTALLATION MUST MEET THE CURRENT CITY OF MEMPHIS STANDARD CONSTRUCTION STANDARDS.

CONTRACTOR SHALL KEEP THE WORK AREA AS CLEAN AS POSSIBLE DURING THE CONSTRUCTION PERIOD AND CLEAN THE WORK AREAS DAILY.

THE CONTRACTOR MUST HAVE WRITTEN APPROVAL FROM THE PROJECT ENGINEER, THE OWNER, AND MATA BEFORE ANY CHANGE IN DESIGN IS MADE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE UTILITY COMPANIES WHICH MAINTAIN A UTILITY LINE WITHIN THE BOUNDARIES OF THE PROJECT PRIOR TO THE INITIATION OF ANY CONSTRUCTION ON THE PROJECT OR IN THE STREETS BORDERING THE PROJECT. THE CONTRACTOR SHALL ALSO ASSUME FULL RESPONSIBILITY FOR DAMAGE TO ANY UTILITIES ENCOUNTERED WITHIN CONSTRUCTION PERIMETERS, WHETHER SHOWN ON THE CONSTRUCTION PLANS OR NOT, DURING THE WORK ON THE PROJECT. FOR SITE LOCATION OF EXISTING UTILITIES INVOLVING MLG&W, SOUTH CENTRAL BELL, AND/OR TEXAS GAS COMPANY, CALL 1-800-351-1111. FOR SEWER LOCATIONS CALL (901) 636-6500.

A MINIMUM OF 24-HOURS PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE CITY OF MEMPHIS CONSTRUCTION INSPECTION OFFICE AT (901) 636-2462.

THE CONTRACTOR SHALL NOTIFY MATA REPRESENTATIVES FRANK HAUSER AND KARL JOHNSON (901) 577-2653 PRIOR TO ANY WORK THAT WILL INVOLVE OR IMPACT THE TRACK, TROLLEY OPERATIONS, TROLLEY STATIONS, OR THE CATENARY OVERHEAD SYSTEM.

THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE RAILS FROM ALL CONSTRUCTION ACTIVITY. CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING, AT HIS EXPENSE, ANY DAMAGE TO EXISTING RAILS, TRACK SLAB, SPECIAL TRACKWORK, ISOLATION MATERIALS, RAIL BOOT, CURBS, OR ANY OTHER MATA PROPERTY. REPAIRS SHALL BE MADE TO THE SATISFACTION OF MATA.

THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES INTENDED TO REMAIN IN SERVICE. ANY EXISTING UTILITY OR UNDERGROUND STRUCTURE TO REMAIN, WHETHER SHOWN ON THE PLANS OR NOT, WHICH IS DAMAGED BY THE CONTRACTORS OPERATIONS, SHALL BE REPAIRED OR REPLACED IN KIND AT NO ADDITIONAL COST TO THE OWNER. THE ACCIDENTAL DISCONNECTION OF ANY UTILITY SERVICE SHALL BE IMMEDIATELY RESTORED AT NO ADDITIONAL COST TO THE OWNER.

THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF SITE FEATURES BASED ON THE CONTRACTORS HORIZONTAL AND VERTICAL CONTROLS. IF, DURING CONSTRUCTION, THE CONTRACTOR FINDS ANY DISCREPANCIES OR CONFLICTS BETWEEN THE PROPOSED SITE IMPROVEMENTS INDICATED ON THE PLANS AND THE PHYSICAL CONDITIONS OF THE SITE, OR ANY ERRORS OR OMISSIONS WITHIN THE PLANS OR IN THE SITE LAYOUT AS PROVIDED BY THE ENGINEER, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IMMEDIATELY NOTIFY THE ENGINEER. ANY DAMAGE OR REWORK CAUSED BY POSSIBLE DISCREPANCIES WILL BE SOLELY THE RESPONSIBILITY OF THE CONTRACTOR.

CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING LOCATION OF UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE UTILITY COMPANIES WHICH MAINTAIN A UTILITY LINE WITHIN THE BOUNDARIES OF THE PROJECT PRIOR TO INITIATION OF ANY CONSTRUCTION ON THE PROJECT OR IN THE STREETS BORDERING THE PROJECT. THE CONTRACTOR SHALL ALSO ASSUME FULL RESPONSIBILITY FOR DAMAGE TO ANY UTILITIES ENCOUNTERED WITHIN CONSTRUCTION PARAMETERS, WHETHER SHOWN ON THE CONSTRUCTION PLANS OR NOT, DURING THE WORK ON THE PROJECT. DAMAGE BY THE CONTRACTORS OPERATIONS SHALL BE REPAIRED OR REPLACED IN KIND AT NO ADDITIONAL COST TO THE OWNER. THE ACCIDENTAL DISCONNECTION OF ANY UTILITY SERVICE SHALL BE IMMEDIATELY RESTORED AT NO ADDITIONAL COST TO THE OWNER. BEFORE EXCAVATIONS ARE BEGUN, THE TENNESSEE ONE-CALL SYSTEM (800) 351-1111 SHOULD BE CONTACTED FOR VERIFICATION OF ALL UTILITY TYPES AND FOR LOCATIONS. NOT ALL UTILITY COMPANIES ARE MEMBERS OF THE TENNESSEE ONE-CALL SYSTEM, THEREFORE, THE CONTRACTOR IS ADVISED TO CONTACT ALL NON-MEMBER UTILITY COMPANIES AS WELL AS THE TENNESSEE ONE-CALL SYSTEM.

GENERAL CONSTRUCTION NOTES:

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES THROUGHOUT CONSTRUCTION.

CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFE AND ADEQUATE WORKING CONDITIONS THROUGHOUT THE DURATION OF CONSTRUCTION ON THE PROPOSED SITE.

ALL CONSTRUCTION MATERIALS AND PROCEDURES SHALL MEET OR EXCEED THE REQUIREMENTS OF THE MEMPHIS AREA TRANSIT AUTHORITY CONSTRUCTION SPECIFICATION AND REQUIREMENTS.

ALL CONSTRUCTION MATERIALS AND PROCEDURES SHALL MEET OR EXCEED THE REQUIREMENTS OF THE CITY OF MEMPHIS STANDARDS.

CONTRACTOR IS RESPONSIBLE FOR REPAIR AND REPLACEMENT OF ANY DAMAGED UTILITIES.

CONTRACTOR IS RESPONSIBLE FOR REPAIR AND REPLACEMENT OF ANY DAMAGED FIBER OPTIC CABLE.

CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

THE CONTRACTOR AT NO TIME SHALL ENCROACH UPON OR CAUSE DISRUPTIONS TO TRAFFIC FLOW ON ADJACENT STREETS RIGHT-OF-WAY WITHOUT SECURING THE PROPER PERMITS PRIOR TO COMMENCING DEMOLITION OR CONSTRUCTION OPERATIONS.

THE CONTRACTOR IS FULLY RESPONSIBLE FOR TRAFFIC CONTROL THROUGHOUT THE DURATION OF CONSTRUCTION. ALL TRAFFIC CONTROL DEVICES AND THEIR INSTALLATION SHALL MEET THE STANDARD PRESCRIBED IN THE MUTCD.

THE APPROPRIATE TRAFFIC CONTROL SHALL BE INSTALLED AT THE INCEPTION OF EACH STAGE OF

CONSTRUCTION AND SHALL BE PROPERLY MAINTAINED AND/OR OPERATED DURING THE TIME SUCH SPECIAL CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS THEY ARE NEEDED AND SHALL BE IMMEDIATELY REMOVED THEREAFTER.

ONE TRAFFIC LANE MUST BE OPEN IN EACH DIRECTION TO MAINTAIN TWO-WAY TRAFFIC AND ALL INAPPROPRIATE EXISTING SIGNS SHALL BE COVERED. LANE ENCROACHMENTS SHALL PERMIT A REMAINING LANE WIDTH OF 10' MINIMUM.

THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC CONTROL ON N. MAIN ST. AND SURROUNDING STREETS AT ALL TIMES WHEN HIS/HER SUBCONTRACTORS ARE WORKING IN THE STREETS.

CONTRACTOR SHALL USE POSITIVE PROTECTION (CONCRETE BARRIERS, ETC.) TO SEPARATE TRAFFIC FROM THE CONSTRUCTION AREA AND PROTECT WORKERS AND MATERIALS.

CONTRACTOR SHALL BE REQUIRED TO NOTIFY THE CITY OF MEMPHIS CONSTRUCTION INSPECTION DEPARTMENT (901.363.2462) AND TRAFFIC ENGINEERING DEPARTMENT (901.576.6710) A MINIMUM OF 24 HOURS PRIOR TO COMMENCING CONSTRUCTION OR IMPLEMENTING A TEMPORARY TRAFFIC CONTROL PLAN. ALL TRAFFIC CONTROL DEVICES MUST BE IN PLACE BEFORE CONSTRUCTION ACTIVITY BEGINS.

SIZES OF ALL SIGNS SHALL COMPLY WITH THE MUTCD.

CONTRACTOR IS RESPONSIBLE FOR INSTALLING THE APPROPRIATE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT MUTCD.

THE CONTRACTOR IS RESPONSIBLE FOR DEMOLITION AREAS THROUGHOUT THE DURATION OF CONSTRUCTION. ANY AREAS OF DEMOLISHED SLAB GREATER THAN 1.5" SHOULD BE REPAIRED IMMEDIATELY OR SHOULD BE PROTECTED OR COVERED TO ELIMINATE ANY HAZARD TO TRAFFIC.

ANY CLOSURE OF THE RIGHT OF WAY SHALL BE TIME LIMITED TO ACTIVE DEMOLITION OR CONSTRUCTION. CONTINUOUS UNWARRANTED CLOSURE OF THE RIGHT OF WAY SHALL NOT BE ALLOWED FOR THE DURATION OF THE PROJECT. IF THIS IS THE CASE, THE CONTRACTOR SHALL PROVIDE AN ADDITIONAL TRAFFIC CONTROL PLAN, SIGNED AND SEALED BY A REGISTERED ENGINEER IN THE STATE OF TENNESSEE, WITH THE TIME NEEDED PER PHASE TO COMPLETE THAT PORTION OF THE WORK. TIME LIMITS WILL BEGIN ON THE DAY OF CLOSURE AND WILL BE MONITORED BY THE ENGINEERING CONSTRUCTION INSPECTORS (901.636.2462) ON THE JOB.

CONTRACTOR MUST ENSURE HE OBTAINS THE NECESSARY PERMITS FROM MATA (1) ONE WEEK MINIMUM PRIOR TO COMMENCEMENT OF WORK.

CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING WITH MATA IN ORDER TO OBTAIN THE ROADWAY WORKER PROTECTION TRAINING.

CONTRACTOR MUST ALWAYS HAVE THE ROW SAFETY IDENTIFICATION CARD ON HIM/HER WHILE HE/SHE IS ON MATA PROPERTY.

FOR FURTHER CLARIFICATION OF WORK INCLUDED UNDER QUANTITIES ON BID FORM, CONSULT CITY OF MEMPHIS CONSTRUCTION SPECIFICATIONS.

GENERAL STRUCTURAL REPAIR & COATING NOTES:

CONTRACTOR SHALL REMOVE ALL SPALLED AND LOOSE CONCRETE ON SLABS AS INDICATED ON THE SUBSEQUENT PLANS. ALL RUSTING AND CORRODED REBAR ENCOUNTERED DURING THE REPAIR PROCESS SHALL BE CLEANED OF RUST.

REPAIR TYPES WERE PREPARED FROM AN SSR INSPECTION OF THE TRACK. IT IS THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL DEFICIENCIES IN THE TRACK AND REPAIR THEM IN ACCORDANCE WITH THE REPAIR TYPE INDICATED IN THESE PLANS. IF THE QUANTITY OF REPAIRS IDENTIFIED BY THE CONTRACTOR EXCEEDS WHAT IS STATED IN THE PLANS, THE CONTRACTOR SHOULD MEET WITH MATA AND THE ENGINEER TO IDENTIFY ADDITIONAL REPAIRS FOR APPROVAL. ALL APPROVED ADDITIONAL REPAIRS WILL BE PAID BY UNIT PRICE SET BY CONTRACTOR IN BID FORM.

CONTRACTOR IS TO NOTIFY THE ENGINEER OF ANY STRUCTURAL DEFICIENCY FOR WHICH THERE IS NOT A REPAIR INDICATED IN THESE PLANS.

CONTRACTOR IS TO FOLLOW THE REPAIR PRODUCT APPLICATION INSTRUCTIONS AND PREPARE THE EXISTING SURFACE IN ACCORDANCE WITH THE PRODUCT REQUIREMENT EVEN IF THIS REQUIRES AN ADDITIONAL PRODUCT OR ADDITIONAL SURFACE PREPARATION.

CONTRACTOR IS TO FOLLOW PROTECTIVE COATING PRODUCT APPLICATION INSTRUCTIONS AND PREPARE THE EXISTING SURFACE THAT IS TO RECEIVE THE PROTECTIVE COATING IN ACCORDANCE WITH THE SURFACE PREPARATION REQUIREMENTS IN THE PRODUCT APPLICATION INSTRUCTIONS.

PRODUCT SUBSTITUTION REQUESTS WILL ONLY BE REVIEWED DURING THE BIDDING PHASE OF THE PROJECT. ANY PRODUCT SUBSTITUTION THAT WAS NOT APPROVED PRIOR TO THE BID DATE WILL NOT BE REVIEWED OR ACCEPTED UNLESS ORIGINALLY SPECIFIED PRODUCTS ARE NOT REASONABLY AVAILABLE FROM PRODUCT MANUFACTURER.

ALL CONSTRUCTION MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE LATEST STATE AND LOCAL GOVERNMENT CONSTRUCTION STANDARDS AND SPECIFICATIONS.

IF, DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR FINDS ANY DISCREPANCIES OR CONFLICTS BETWEEN THE PROPOSED SITE IMPROVEMENTS INDICATED ON THE PLANS AND THE PHYSICAL CONDITIONS OF THE SITE, OR ANY ERRORS OR OMISSIONS WITHIN THE PLANS OR IN THE SITE LAYOUT AS PROVIDED BY THE ENGINEER, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IMMEDIATELY NOTIFY THE ENGINEER. UNTIL AUTHORIZED TO PROCEED, ANY WORK PERFORMED BY THE CONTRACTOR AFTER SUCH A DISCOVERY WILL BE AT THE CONTRACTOR'S SOLE RISK AND EXPENSE.

ALL HANDICAP ACCESSIBLE AREAS OF THE SITE SHALL BE RETURNED TO ADA COMPLIANCE UPON COMPLETION OF CONSTRUCTION. THIS MUST MEET ANY FEDERAL, STATE, AND LOCAL CODES AND REQUIREMENTS.

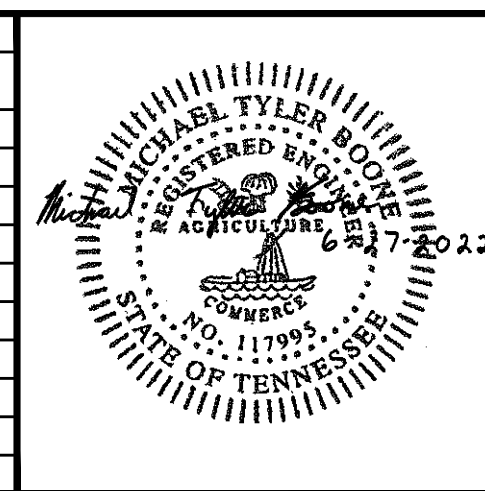
ALL NEW ASPHALT AND/OR CONCRETE PATCHING (INCLUDING SIDEWALK) SHALL MATCH EXISTING PAVEMENTS FLUSH.

CONTRACTOR SHALL REPAIR, AT HIS EXPENSE, ANY DAMAGE TO EXISTING ASPHALT, CONCRETE, CURBS, RAILS, SIDEWALKS, SIGNAGE, TRACK SLAB, RAIL BOOT, RAIL, ETC. RESULTING FROM CONSTRUCTION TRAFFIC AND/OR OPERATIONS. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE OWNER AND/OR ENGINEER.

CONTRACTOR SHALL REPLACE ANY EXISTING TRAFFIC STRIPING DESTROYED BY REPAIR OPERATIONS IN KIND.

REPAIR WORK INCLUDES REPLACING ASPHALTIC CONCRETE SURFACE WITH POLYMER MODIFIED CONCRETE OVERLAY ON N. MAIN ST. FROM JUST SOUTH OF INTERSECTION WITH SHADYAC AVE. TO JUST NORTH OF INTERSECTION WITH MILL AVE.

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



DRAINAGE BASIN: GAYOSO 1-G

C1.0

SHEET 1 OF 10

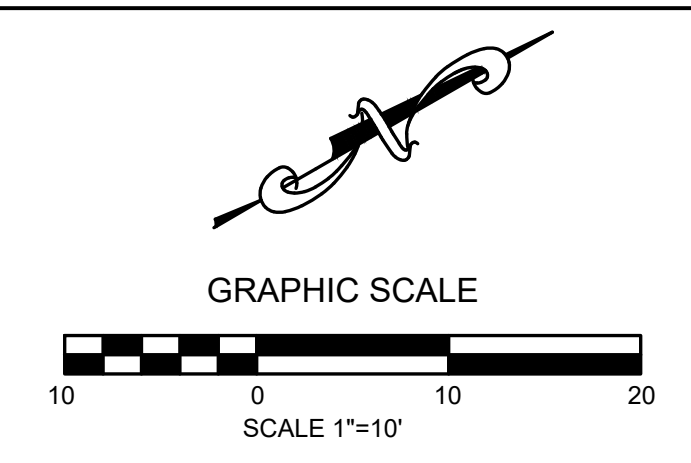
GENERAL NOTES

LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

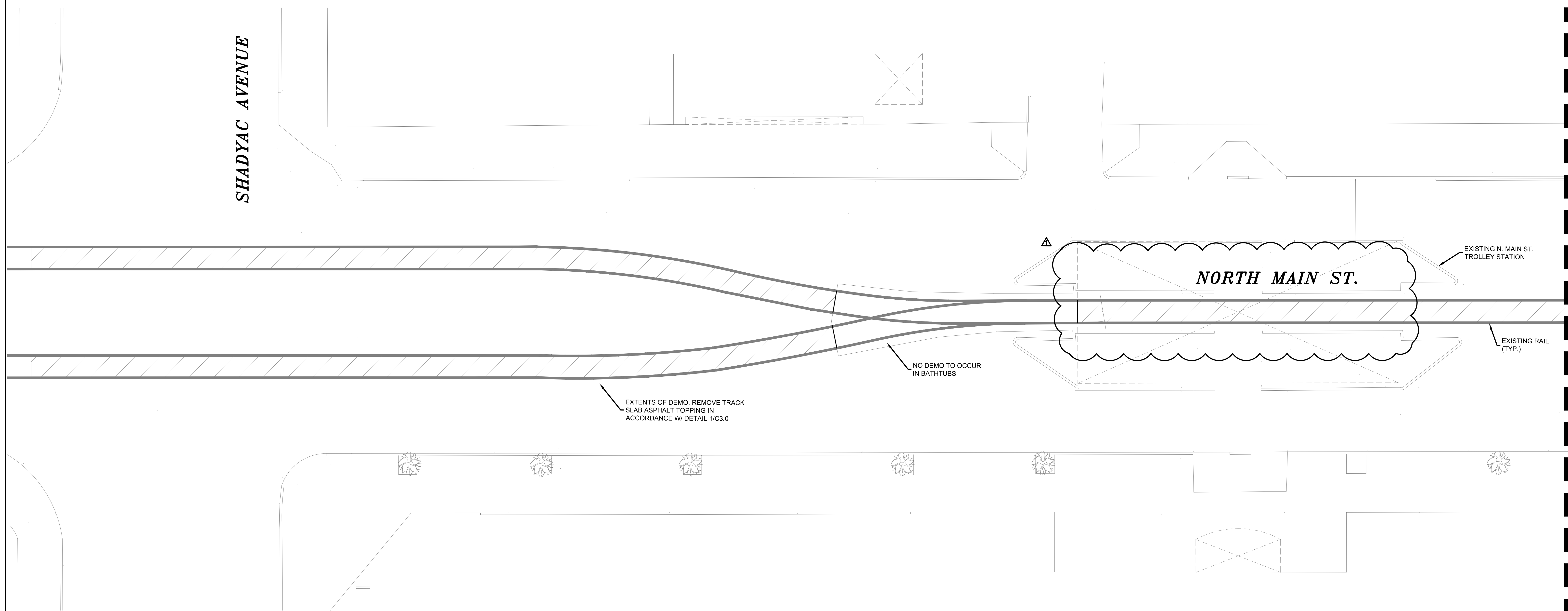
SURVEY DESIGN BY N/A SSR, INC. DATE MARCH 2022 DATE N/A SCALE AS NOTED

DEPUTY CITY ENGINEER _____ DATE _____ CITY ENGINEER _____ DATE _____

TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

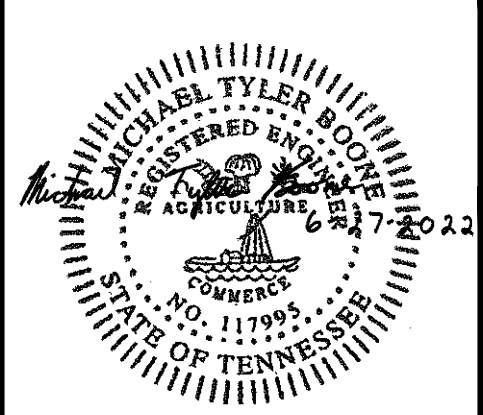


SHADYAC AVENUE



MATCHLINE SEE SHEET C1.2

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.
1	3-9-2022	MTB	ADDED AREA		



TRACK SLAB PAVEMENT REPAIR
 NORTH MAIN ST.
 DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
 ENGINEER: SSR, INC.

DRAINAGE BASIN: GAYOSO 1-G

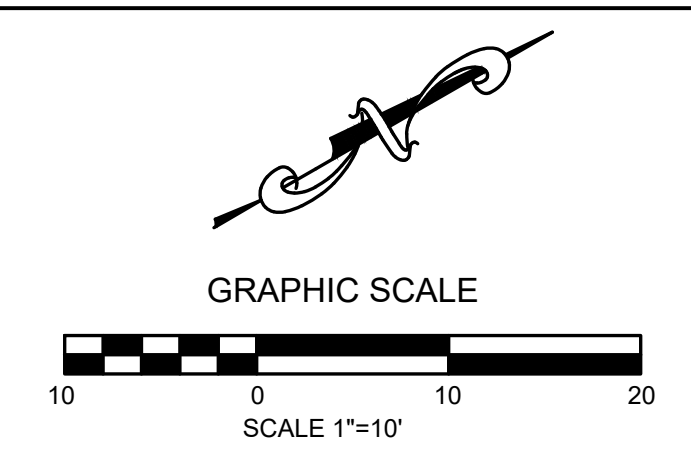
SHEET 2 OF 10

N MAIN ST. EXISTING CONDITIONS AND DEMOLITION PLAN

LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

SURVEY: N/A DATE: N/A
 DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
 REVIEWED: _____
 DEPUTY CITY ENGINEER _____ DATE _____ CITY ENGINEER _____ DATE _____

C1.1



MATCHLINE SEE SHEET C1.1

MATCHLINE SEE SHEET C1.3

A.W. WILLIS AVENUE

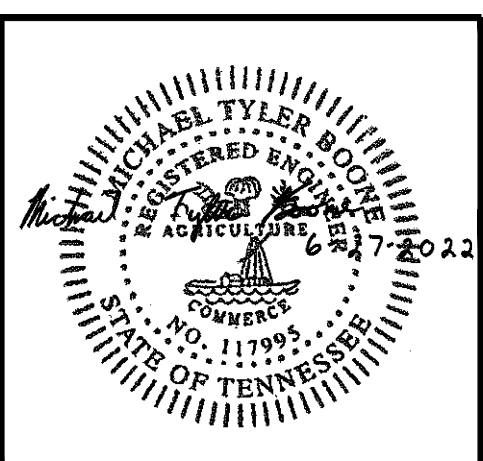
NORTH MAIN ST.

EXTENTS OF DEMO. REMOVE TRACK
SLAB ASPHALT TOPPING IN
ACCORDANCE W/ DETAIL 1/C3.0

NO DEMO TO OCCUR
IN BATHTUBS

EXISTING RAIL
(TYP.)

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



DRAINAGE BASIN: GAYOSO 1-G

SHEET 3 OF 10

C1.2

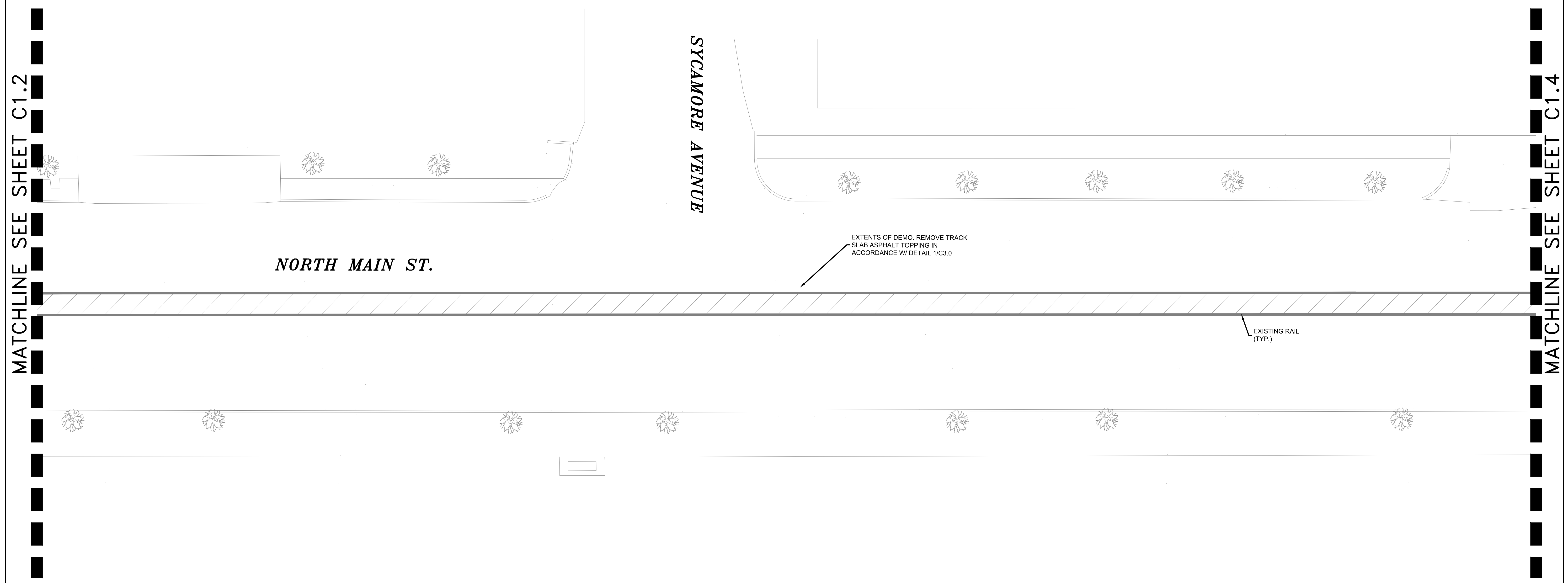
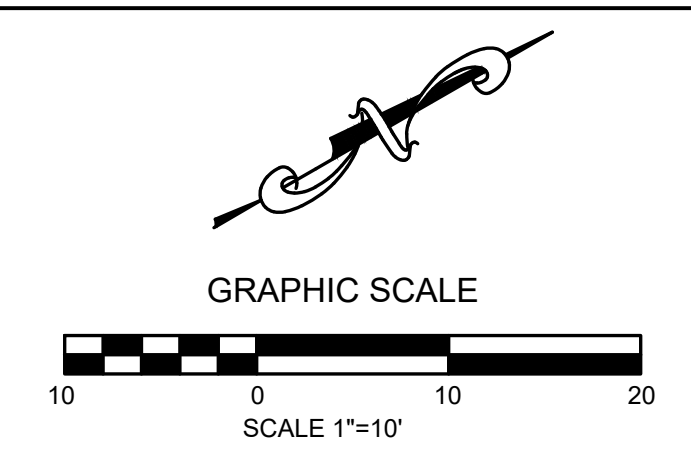
**N MAIN ST. EXISTING CONDITIONS
AND DEMOLITION PLAN**

LOCATION: N MAIN STREET BETWEEN
SHADYAC AVE. AND MILL AVE.
MEMPHIS, TENNESSEE

**TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.**
DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

SURVEY: N/A DATE: N/A
DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
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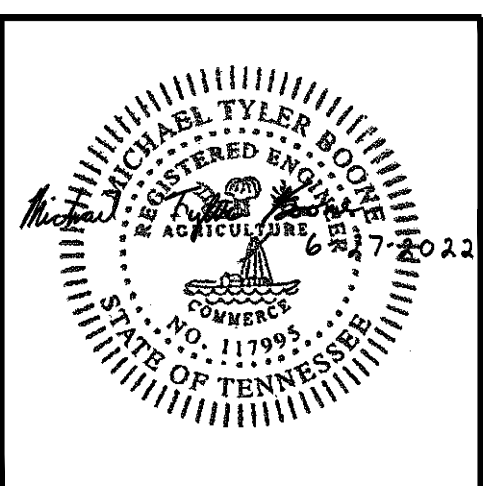
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MATCHLINE SEE SHEET C1.2

MATCHLINE SEE SHEET C1.4

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



DRAINAGE BASIN: GAYOSO 1-G

SHEET 4 OF 10 C1.3

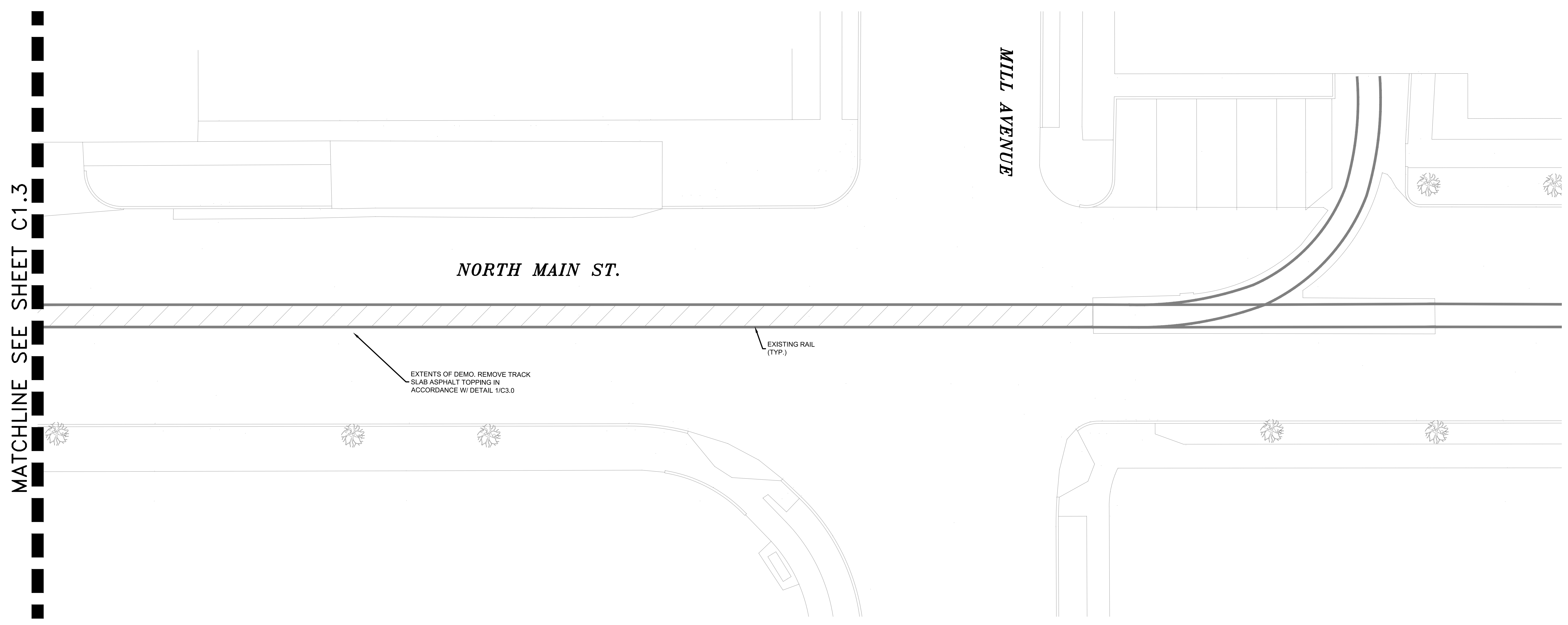
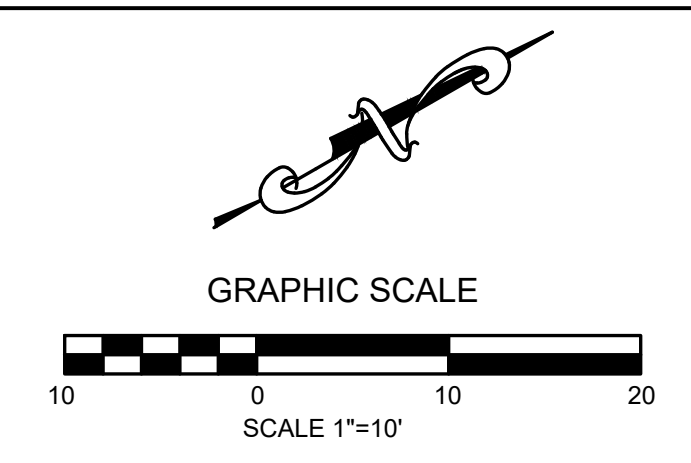
N MAIN ST. EXISTING CONDITIONS AND DEMOLITION PLAN

LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

SURVEY: N/A DATE: N/A
 DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
 REVIEWED: _____

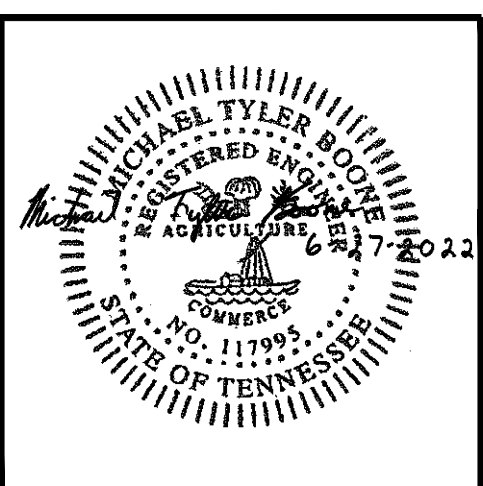
DEPUTY CITY ENGINEER _____ DATE _____ CITY ENGINEER _____ DATE _____

TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
 DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
 ENGINEER: SSR, INC.



MATCHLINE SEE SHEET C1.3

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



TRACK SLAB PAVEMENT REPAIR
 NORTH MAIN ST.
 DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
 ENGINEER: SSR, INC.

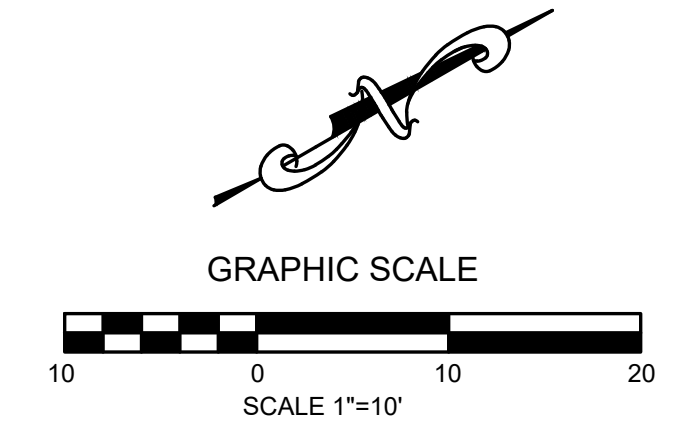
DRAINAGE BASIN: GAYOSO 1-G

SHEET 5 OF 10

C1.4

N MAIN ST. EXISTING CONDITIONS AND DEMOLITION PLAN
 LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

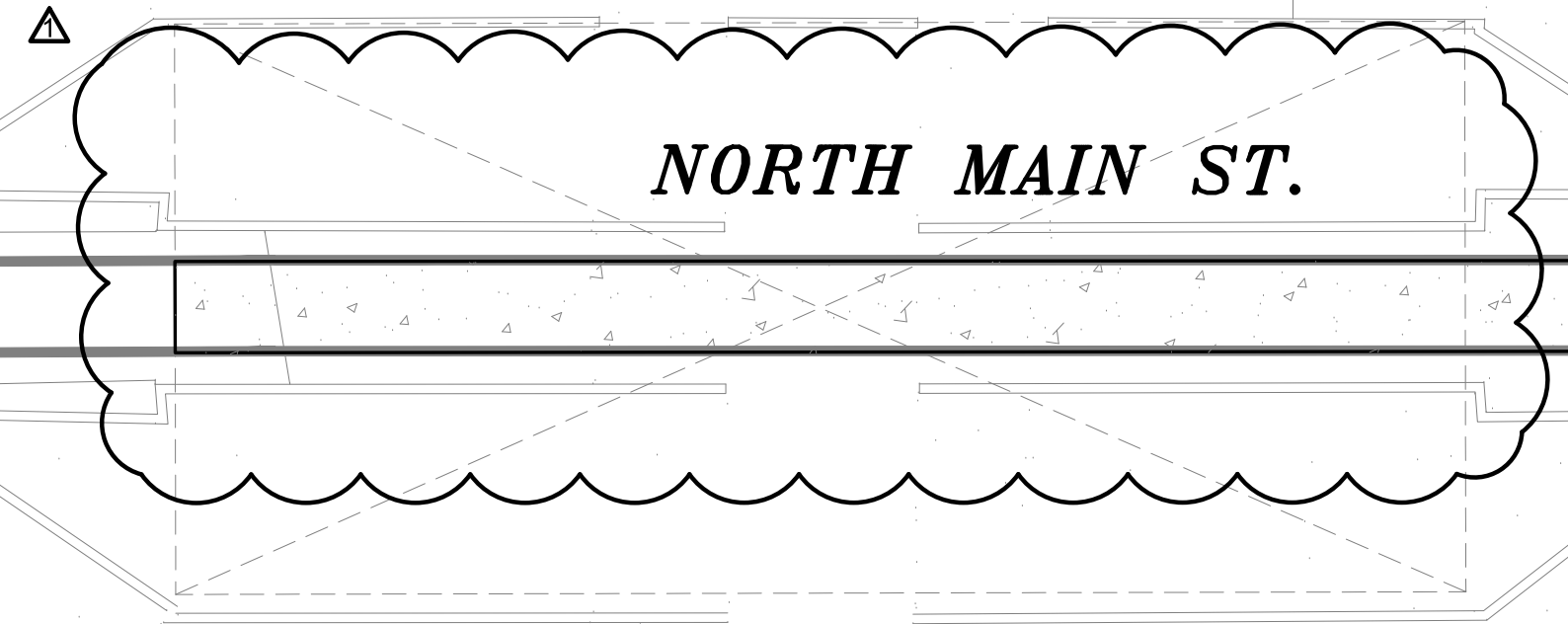
SURVEY: N/A DATE: N/A
 DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
 REVIEWED: _____
 DEPUTY CITY ENGINEER _____ DATE _____ CITY ENGINEER _____ DATE _____



SHADYAC AVENUE

PROPOSED POLYMER MODIFIED CONCRETE
TOPPING SLAB, SEE DETAIL 2/C3.0

NO REPAIR TO
OCCUR IN BATHTUBS

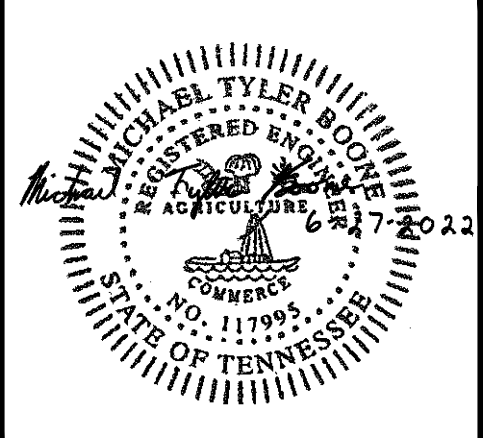


EXISTING N. MAIN ST.
TROLLEY STATION

EXISTING RAIL
(TYP.)

MATCHLINE SEE SHEET C2.2

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.
1	3-9-2022	MTB	ADDED AREA		



DRAINAGE BASIN: GAYOSO 1-G

SHEET 6 OF 10

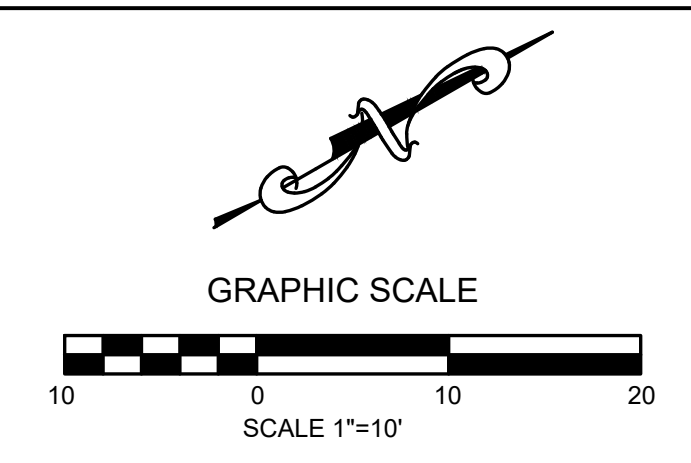
C2.1

**N MAIN ST. PROPOSED
PAVEMENT PLAN**

LOCATION: N MAIN STREET BETWEEN
SHADYAC AVE. AND MILL AVE.
MEMPHIS, TENNESSEE

TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

SURVEY: N/A DATE: N/A
DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
REVIEWED: _____
DEPUTY CITY ENGINEER: _____ DATE: _____ CITY ENGINEER: _____ DATE: _____



MATCHLINE SEE SHEET C2.1

MATCHLINE SEE SHEET C2.3

A.W. WILLIS AVENUE

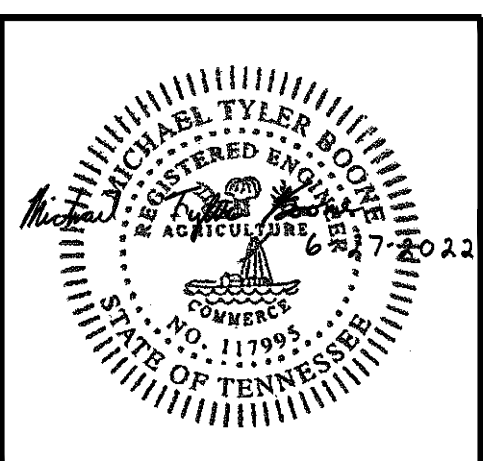
NORTH MAIN ST.

PROPOSED POLYMER MODIFIED CONCRETE
TOPPING SLAB, SEE DETAIL 2/C3.0

NO REPAIR TO
OCCUR IN BATHTUBS

EXISTING RAIL
(TYP.)

REVISIONS				APPROVED	
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DRAINAGE BASIN: GAYOSO 1-G

C2.2

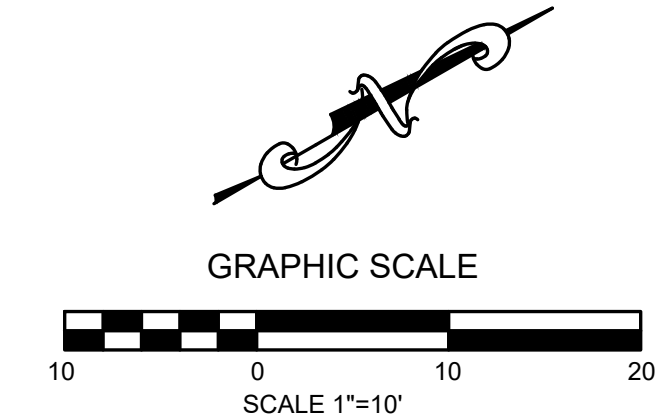
SHEET 7 OF 10

N MAIN ST. PROPOSED PAVEMENT PLAN

LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

SURVEY: N/A
DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
REVIEWED: _____
DEPUTY CITY ENGINEER: _____ DATE: _____ CITY ENGINEER: _____ DATE: _____

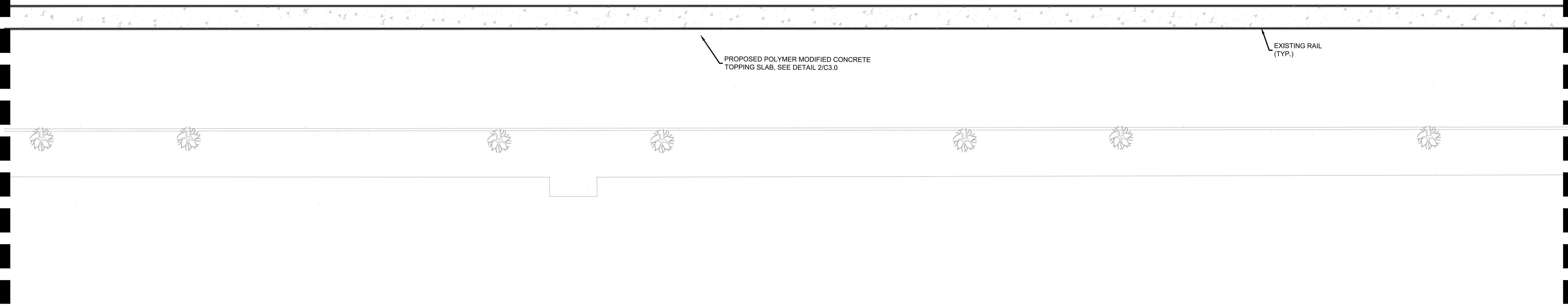
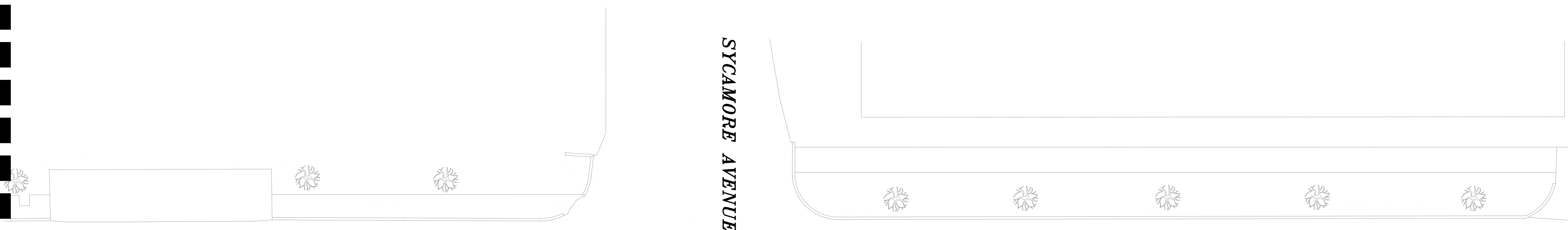


MATCHLINE SEE SHEET C2.2

MATCHLINE SEE SHEET C2.4

SYCAMORE AVENUE

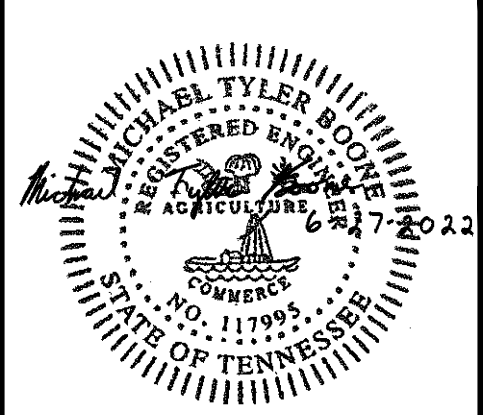
NORTH MAIN ST.



PROPOSED POLYMER MODIFIED CONCRETE
TOPPING SLAB, SEE DETAIL 2/C3.0

EXISTING RAIL
(TYP.)

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



DRAINAGE BASIN: GAYOSO 1-G

C2.3

SHEET 8 OF 10

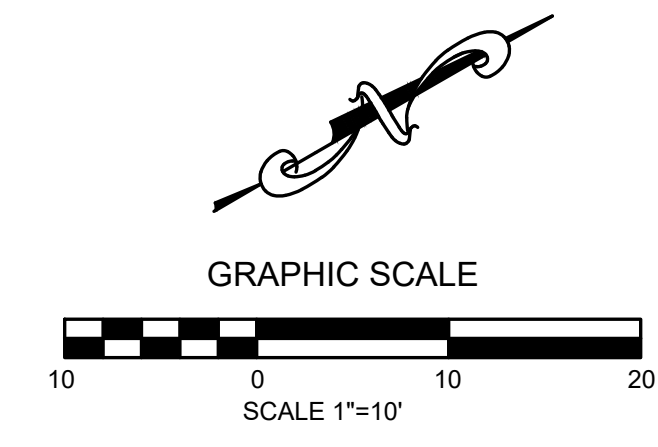
N MAIN ST. PROPOSED PAVEMENT PLAN

LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

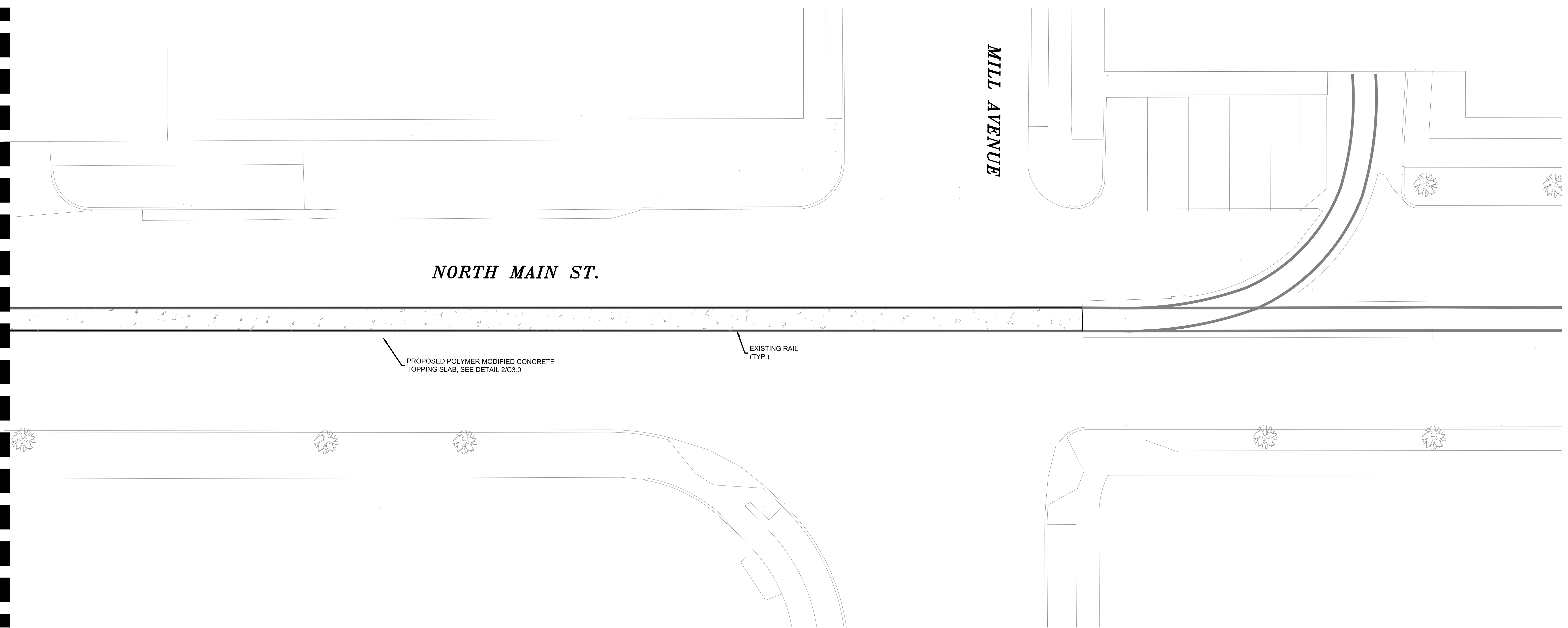
TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

SURVEY: N/A
DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
REVIEWED: _____

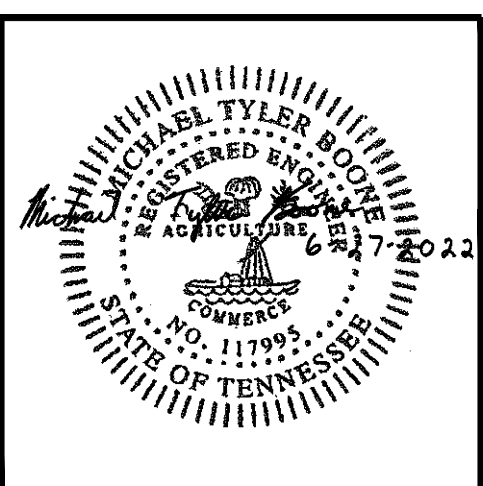
DEPUTY CITY ENGINEER _____ DATE _____ CITY ENGINEER _____ DATE _____



MATCHLINE SEE SHEET C2.3



REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



DRAINAGE BASIN: GAYOSO 1-G

SHEET 9 OF 10

N MAIN ST. PROPOSED PAVEMENT PLAN

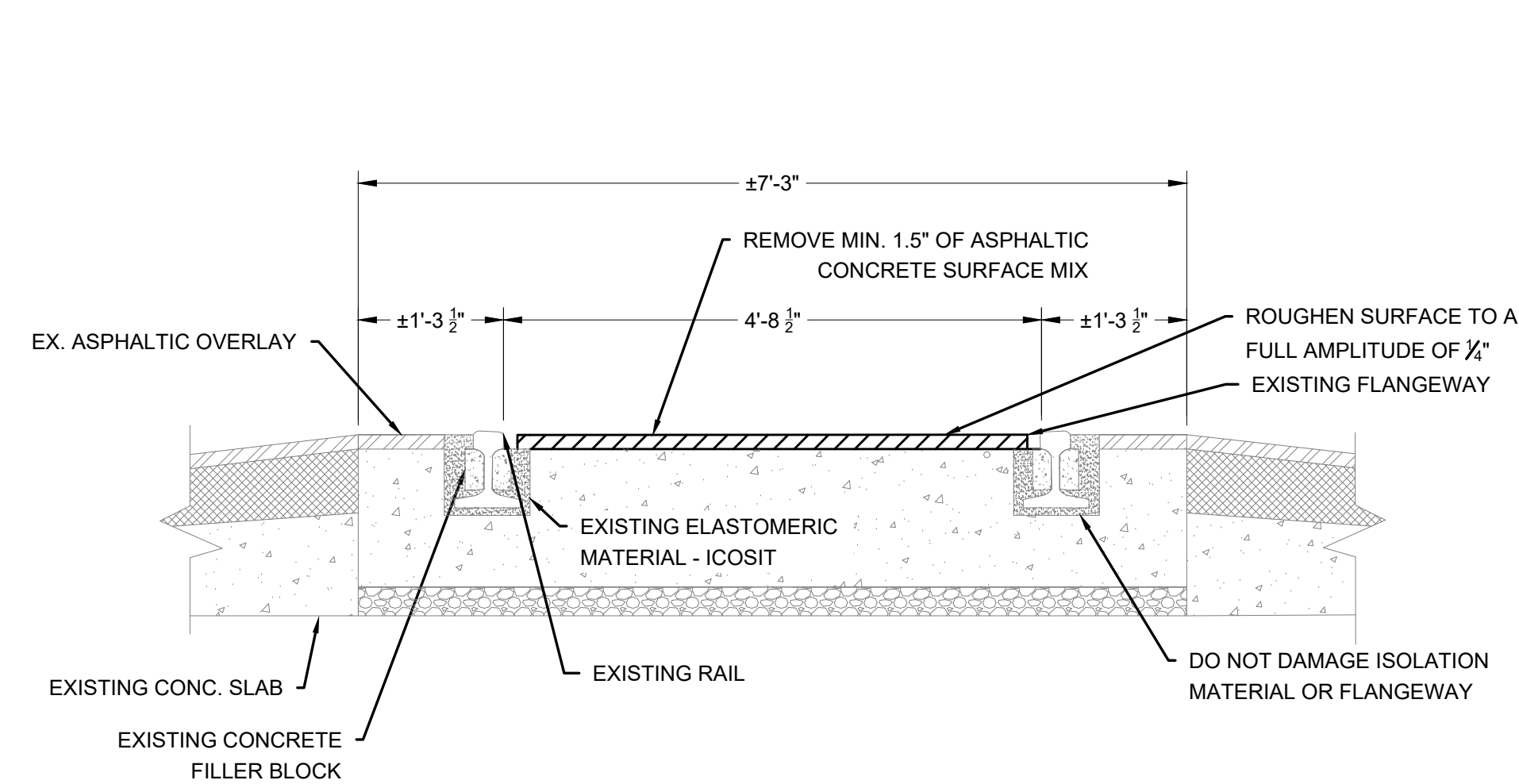
LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

SURVEY: N/A DATE: N/A
 DESIGN BY: SSR, INC. DATE: MARCH 2022 SCALE: 1"=10'
 REVIEWED: _____

DEPUTY CITY ENGINEER _____ DATE _____ CITY ENGINEER _____ DATE _____

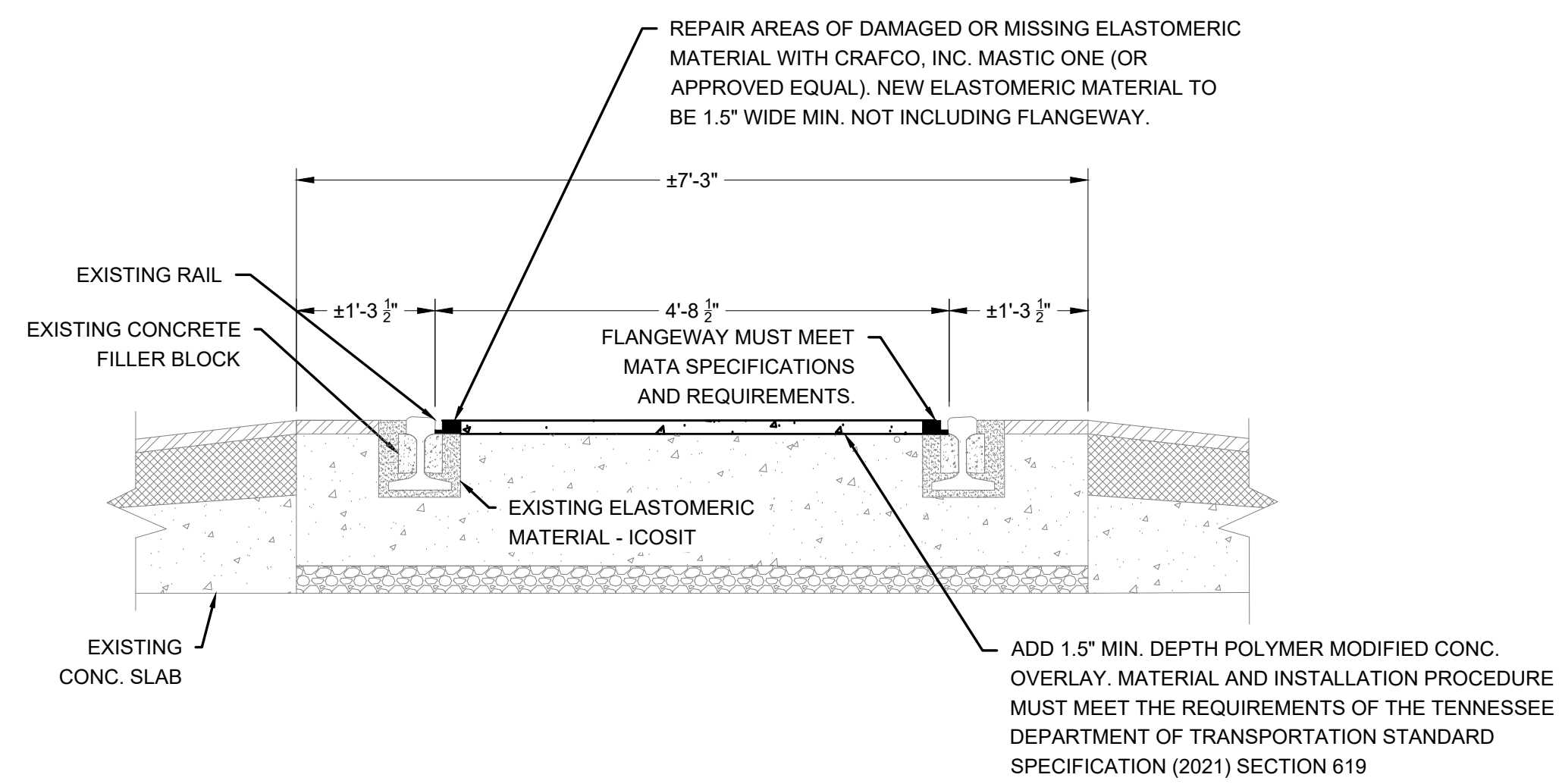
TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
 DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
 ENGINEER: SSR, INC.

C2.4



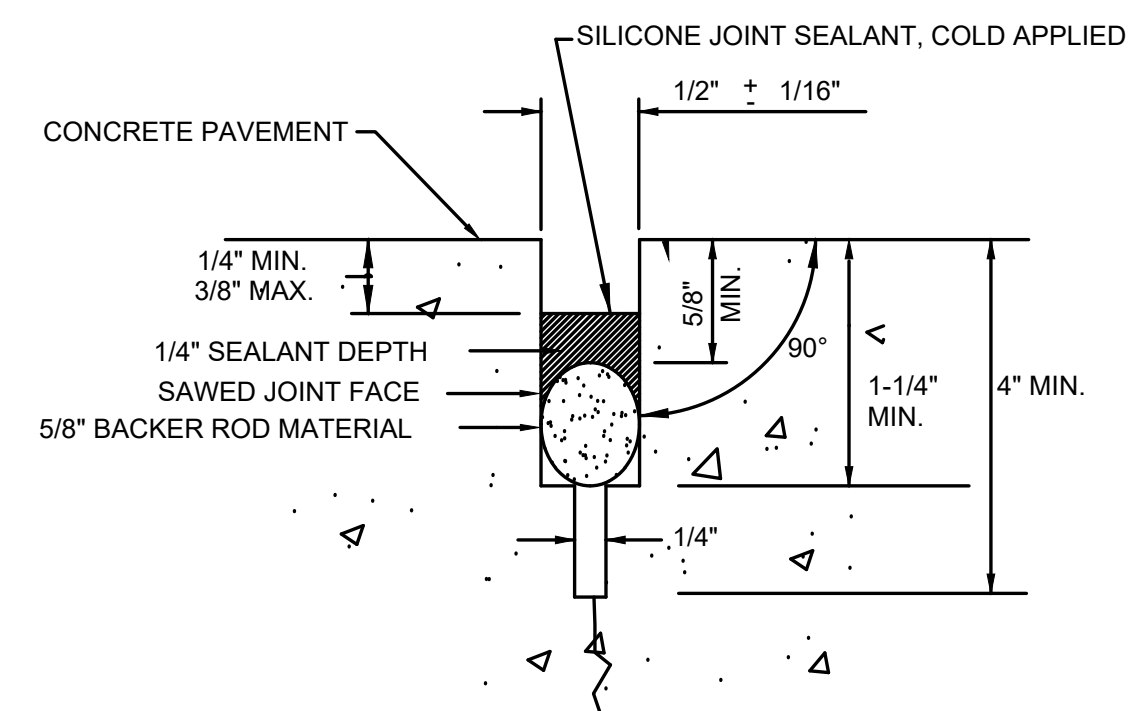
1 TYPICAL DEMOLITION DETAIL
C3.0 NOT TO SCALE

- NOTES:
1. PRIOR TO POLYMER MODIFIED CONCRETE PLACEMENT, CONCRETE MUST BE STRUCTURALLY SOUND AND FREE FROM ASPHALT, DETERIORATION, DUST, DIRT, DEBRIS, PAINT, OIL, OR OTHER DETRITUS.
 2. CONTRACTOR SHALL USE CARE TO ENSURE MINIMAL DAMAGE OCCURS TO EXISTING ELASTOMERIC MATERIAL.
 3. FOLLOWING REMOVAL OF ASPHALTIC MATERIAL, CONTACT ENGINEER FOR INSPECTION OF EXISTING TRACK SLAB.



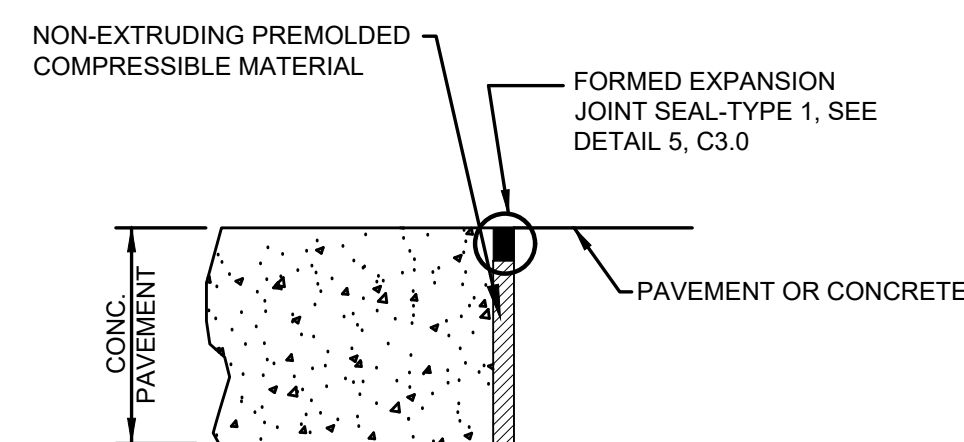
2 TYPICAL REPAIR DETAIL
C3.0 NOT TO SCALE

- NOTES:
1. CONTRACTOR TO MATCH JOINT LAYOUT OF EXISTING TRACK SLAB.
 2. SOME REPAIRS ON FIELD SIDE OF RAIL MAY BE REQUIRED IN AREAS DEEMED NECESSARY BY THE ENGINEER OR OWNER. IN THESE AREAS, SAWCUT ASPHALT SURFACE TO CLEAN LINES AND FOLLOW SAME REPAIR PROCEDURE AS SHOWN FOR INSIDE RAIL.



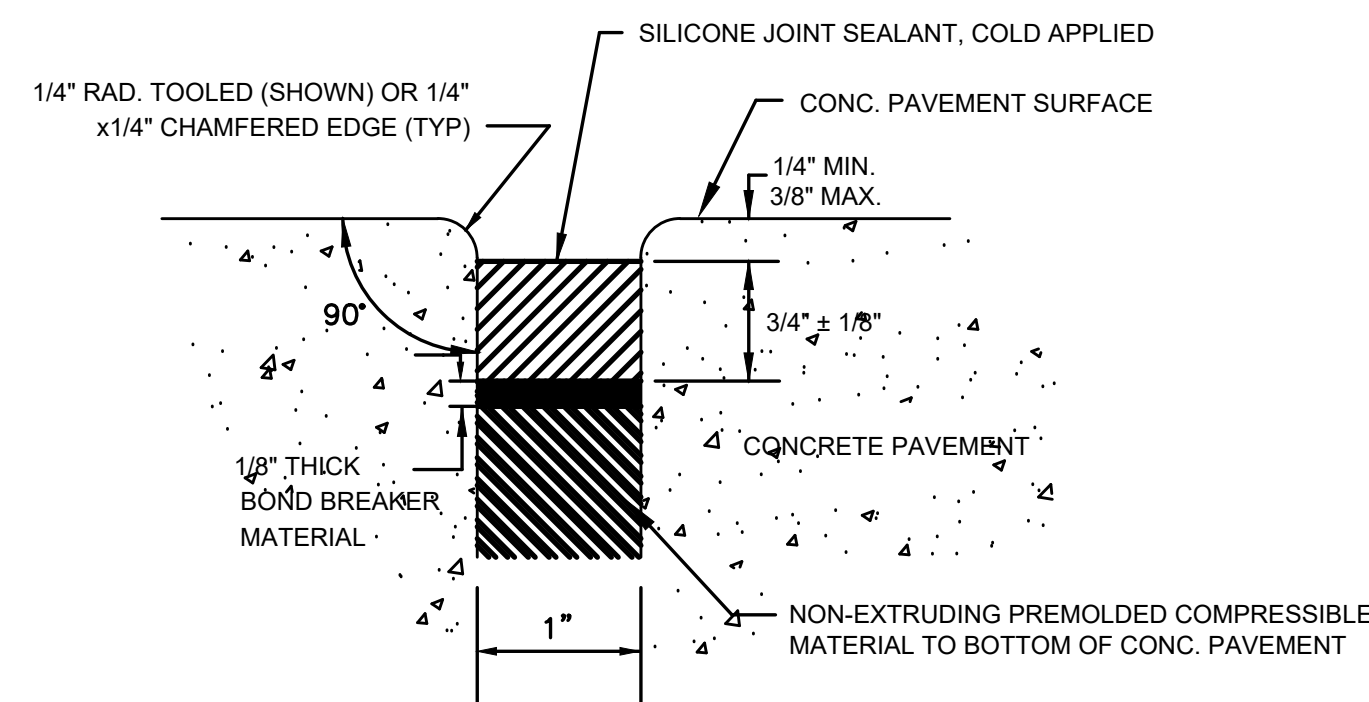
3 CONTRACTION JOINT DETAIL
C3.0 NOT TO SCALE

NOTE: AS NEEDED OR REQUIRED PER ENGINEER'S INSPECTION.



4 EXPANSION JOINT DETAIL
C3.0 NOT TO SCALE

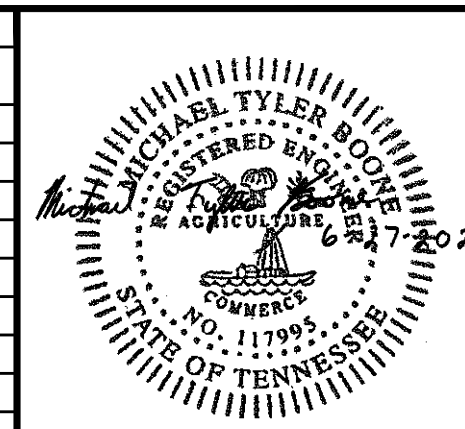
NOTE: AS NEEDED OR REQUIRED PER ENGINEER'S INSPECTION.



5 FORMED EXPANSION JOINT SEAL
C3.0 NOT TO SCALE

NOTE: AS NEEDED OR REQUIRED PER ENGINEER'S INSPECTION.

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

DRAINAGE BASIN: GAYOSO 1-G

SHEET 10 OF 10

C3.0

DETAILS

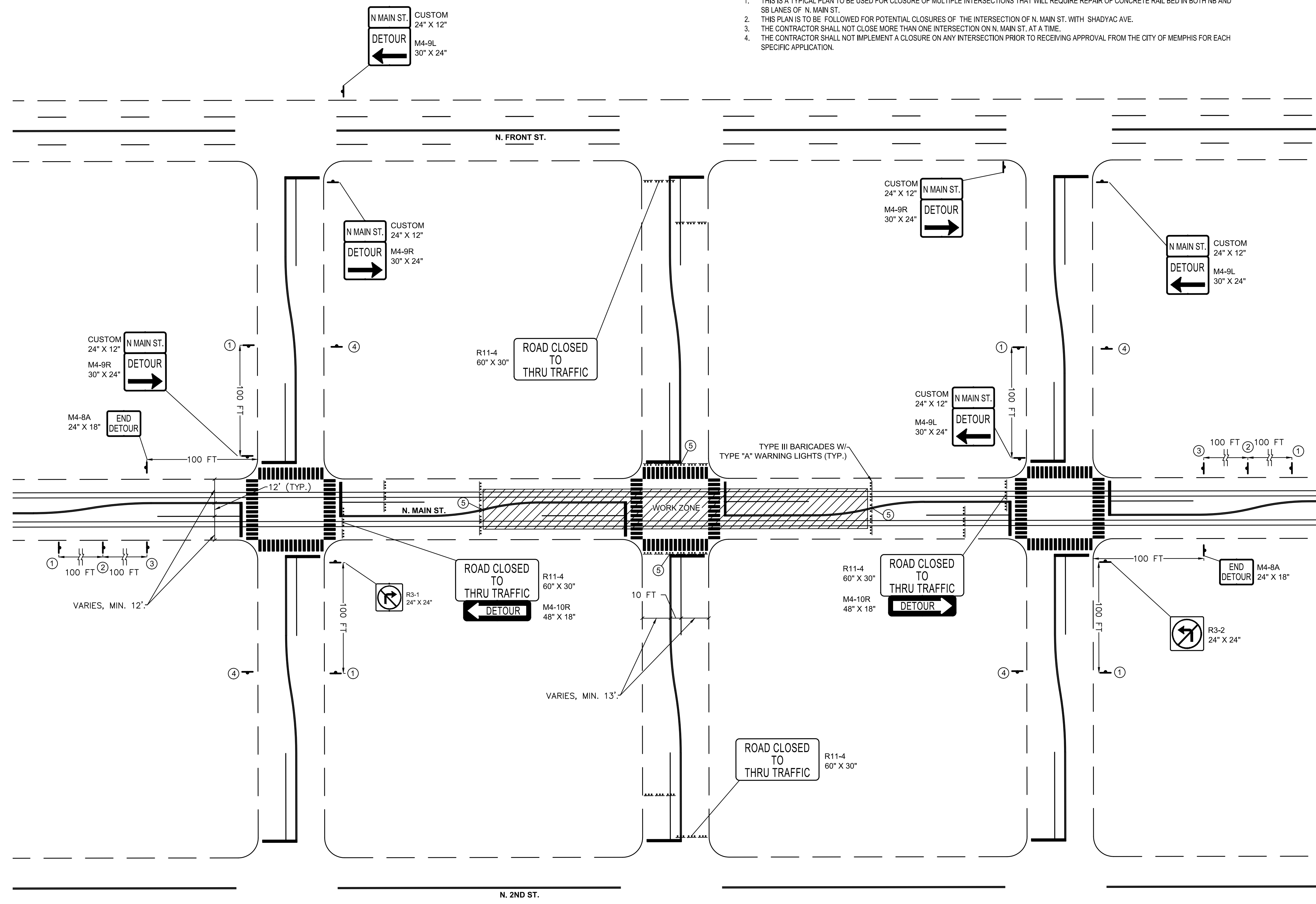
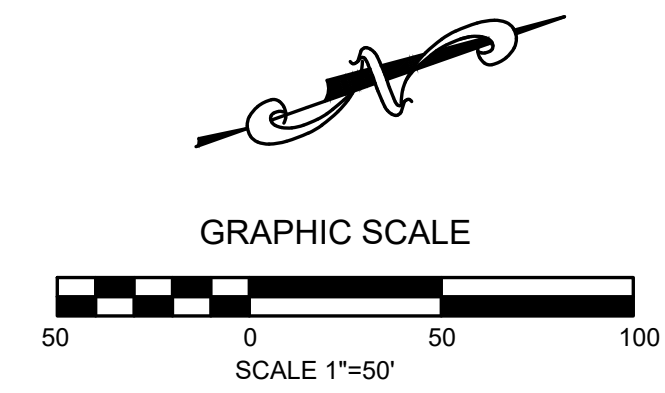
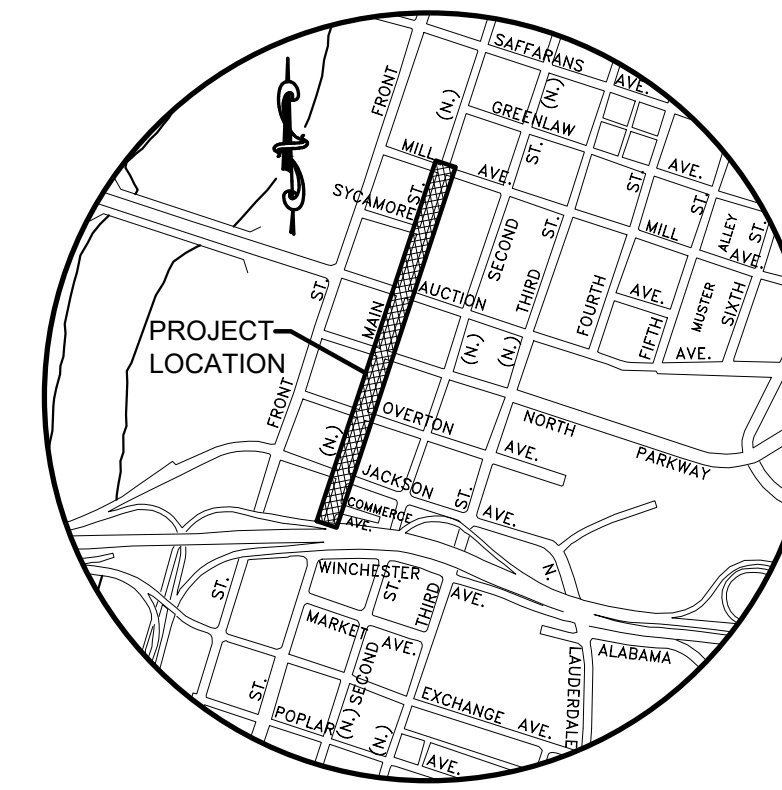
LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

SURVEY: N/A
DESIGN BY: SSR, INC. DATE: MARCH 2022
SCALE: AS NOTED

DEPUTY CITY ENGINEER: _____ DATE: _____ CITY ENGINEER: _____ DATE: _____

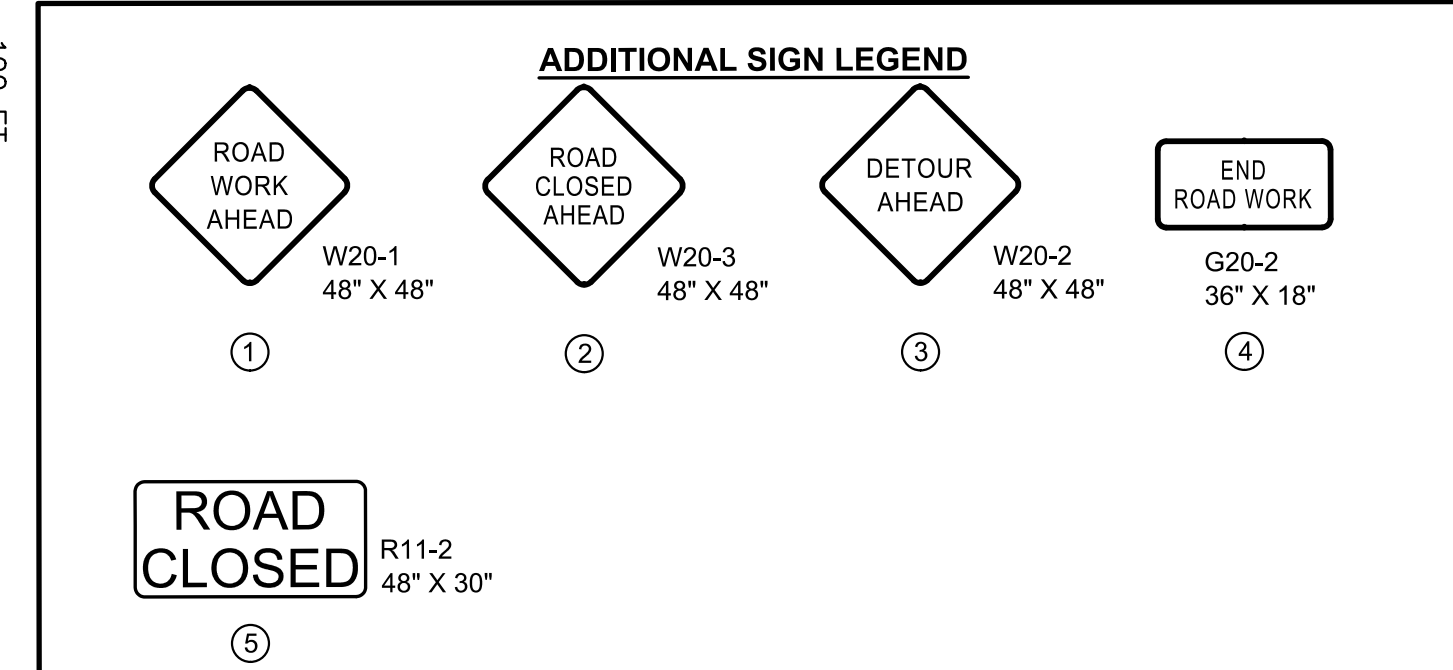
SPECIAL NOTES:

1. THIS IS A TYPICAL PLAN TO BE USED FOR CLOSURE OF MULTIPLE INTERSECTIONS THAT WILL REQUIRE REPAIR OF CONCRETE RAIL BED IN BOTH NB AND SB LANES OF N. MAIN ST.
2. THIS PLAN IS TO BE FOLLOWED FOR POTENTIAL CLOSURES OF THE INTERSECTION OF N. MAIN ST. WITH SHADYAC AVE.
3. THE CONTRACTOR SHALL NOT CLOSE MORE THAN ONE INTERSECTION ON N. MAIN ST. AT A TIME.
4. THE CONTRACTOR SHALL NOT IMPLEMENT A CLOSURE ON ANY INTERSECTION PRIOR TO RECEIVING APPROVAL FROM THE CITY OF MEMPHIS FOR EACH SPECIFIC APPLICATION.

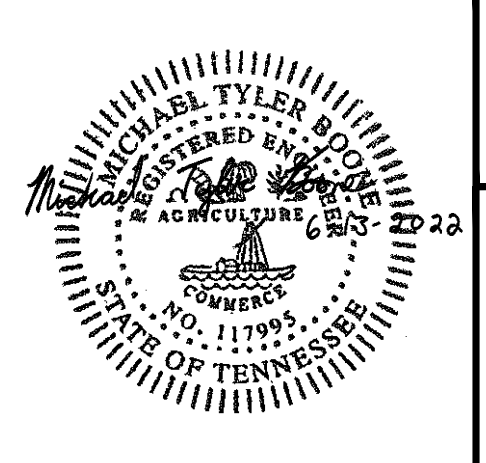


NOTES:

1. SEE SECTION 8F.03, SIGN PLACEMENT, OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR INFORMATION ON PLACEMENT AND MOUNTING OF SIGNS.
2. SIGNS SHOWN ON THIS PLAN ARE TO WARN TRAFFIC ABOUT THE CONSTRUCTION. OTHER TRAFFIC CONTROL DEVICES MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. NOTHING IN THIS PLAN IS INTENDED TO SUPERSEDE OR RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING THE APPROPRIATE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT STATE OF TENNESSEE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS."
4. CONTRACTOR SHALL BE REQUIRED TO NOTIFY THE CITY OF MEMPHIS CONSTRUCTION INSPECTION DEPARTMENT (636-2462) AND TRAFFIC ENGINEERING DEPARTMENT (636-6710) A MINIMUM OF 24 HOURS PRIOR TO COMMENCING CONSTRUCTION OR IMPLEMENTING A TRAFFIC CONTROL PLAN. ALL TRAFFIC CONTROL DEVICES MUST BE IN PLACE BEFORE CONSTRUCTION ACTIVITY BEGINS.
5. SIZES OF ALL SIGNS SHALL COMPLY WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
6. ALL TRAFFIC CONTROL DEVICES AND THEIR INSTALLATION SHALL MEET THE STANDARD PRESCRIBED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND SHALL COMPLY WITH THE STATE OF TENNESSEE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 712 TEMPORARY TRAFFIC CONTROL.
7. ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED WHENEVER POSSIBLE. THE CONTRACTOR IS RESPONSIBLE FOR COMPENSATING PROPERTY OWNERS THAT LOSE ACCESS DURING ROAD CLOSURE.
8. SIDE STREET, DRIVEWAY ACCESS, AND SAFE PEDESTRIAN WAYS SHALL BE MAINTAINED AT ALL TIMES.
9. THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THE RIGHT-OF-WAY OR WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHICHEVER IS LESS, WHEN THE LANE IS OPEN TO TRAFFIC, UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THE RIGHT-OF-WAY OR WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE WHICHEVER IS LESS, AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE.
10. CONTRACTOR SHALL USE PLASTIC DRUMS WITH TYPE "A" WARNING LIGHTS TO SEPARATE TRAFFIC FROM THE CONSTRUCTION AREA.
11. CONTRACTOR SHALL COVER ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SIGNS OR DEVICES DURING CONSTRUCTION AND THEY SHALL REMAIN COVERED DURING CONSTRUCTION AND UNTIL SUCH TIME THAT NO CONFLICT EXISTS.
12. CONTRACTOR SHALL INSTALL TEMPORARY NO PARKING SIGNS AS REQUIRED WITHIN THE TRAFFIC CONTROL AREA.
13. ALL TEMPORARY OR PERMANENT TRAVELED SURFACE SHALL BE INSPECTED DAILY BY THE CONTRACTOR (INCLUDING WEEKENDS) AND NECESSARY PATCHING OR RE-FINISHING PERFORMED.
14. MARKINGS SHALL BE MAINTAINED IN LONG-TERM STATIONARY WORK AREAS AND SHALL MATCH AND MEET THE MARKINGS IN PLACE AT BOTH ENDS OF THE WORK AREA.
15. CONTRACTOR SHALL CONTACT ALL APPROPRIATE AGENCIES BEFORE CLOSING ANY ROADWAYS.
16. CONTRACTOR SHALL CONTACT THE CITY OF MEMPHIS SIGNAL SHOP AT 528-2844 FOR LOCATION OF SIGNAL CONDUIT AND WIRES.
17. THE APPROPRIATE TRAFFIC CONTROL SHALL BE INSTALLED AT THE INCEPTION OF EACH STAGE OF CONSTRUCTION AND SHALL BE PROPERLY MAINTAINED AND/OR OPERATED DURING THE TIME SUCH SPECIAL CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS THEY ARE NEEDED AND SHALL BE IMMEDIATELY REMOVED THEREAFTER.
18. EXISTING STRIPING THAT CONFLICTS WITH THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE COVERED OR REMOVED DURING CONSTRUCTION. WHEN CONSTRUCTION IS COMPLETE THE EXISTING STRIPING SHALL BE RETURNED TO ITS ORIGINAL STATE.
19. ALL TRAFFIC CONTROL SIGNS SHALL MEET THE MINIMUM RETROREFLECTIVITY LEVELS SPECIFIED IN THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
20. THE CONTRACTOR IS PROHIBITED FROM CLOSING THE EXISTING SIDEWALK AT ANYTIME DURING THIS PROJECT. IF A SIDEWALK CLOSURE IS NEEDED, A REVISED TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO AND APPROVED BY THE CITY ENGINEER'S OFFICE.
21. ANY CLOSURE OF THE RIGHT OF WAY SHALL BE TIME LIMITED TO THE ACTIVE DEMOLITION OR CONSTRUCTION. CONTINUOUS UNWARRANTED CLOSURE OF THE RIGHT OF WAY SHALL NOT BE ALLOWED FOR THE DURATION OF THE PROJECT. THE DEVELOPER SHALL PROVIDE ON THE TRAFFIC CONTROL PLAN, THE TIME NEEDED PER PHASE TO COMPLETE THAT PORTION OF THE WORK. TIME LIMITS WILL BEGIN ON THE DAY OF CLOSURE AND WILL BE MONITORED BY THE ENGINEERING CONSTRUCTION INSPECTORS (636-2462) ON THE JOB.
22. THE CONTRACTOR SHALL NOTIFY MEMPHIS AREA TRANSIT AUTHORITY (MATA) AT 722-7100 A MINIMUM OF 48 HOURS PRIOR TO IMPLEMENTING A TRAFFIC CONTROL PLAN.
23. ONLY ONE PHASE OF THE TRAFFIC CONTROL PLAN SHALL BE ACTIVE AT ANY ONE TIME.



REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



DESIGN SPEED LIMIT: 35 MPH
 DATE: TBD
 DURATION: 5 DAYS PER INTERSECTION

TRACK SLAB PAVEMENT REPAIR
 NORTH MAIN ST.
 DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
 ENGINEER: SSR, INC.

TC1.0

SHEET 1 OF 4

**TRAFFIC CONTROL PLAN
 FOR DOUBLE TRACK REPAIR (TYPICAL)**

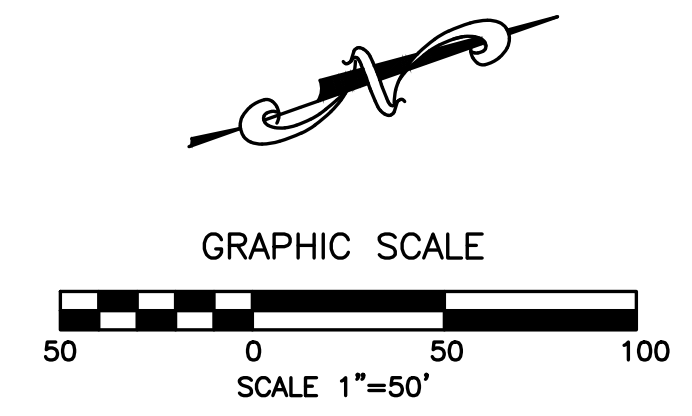
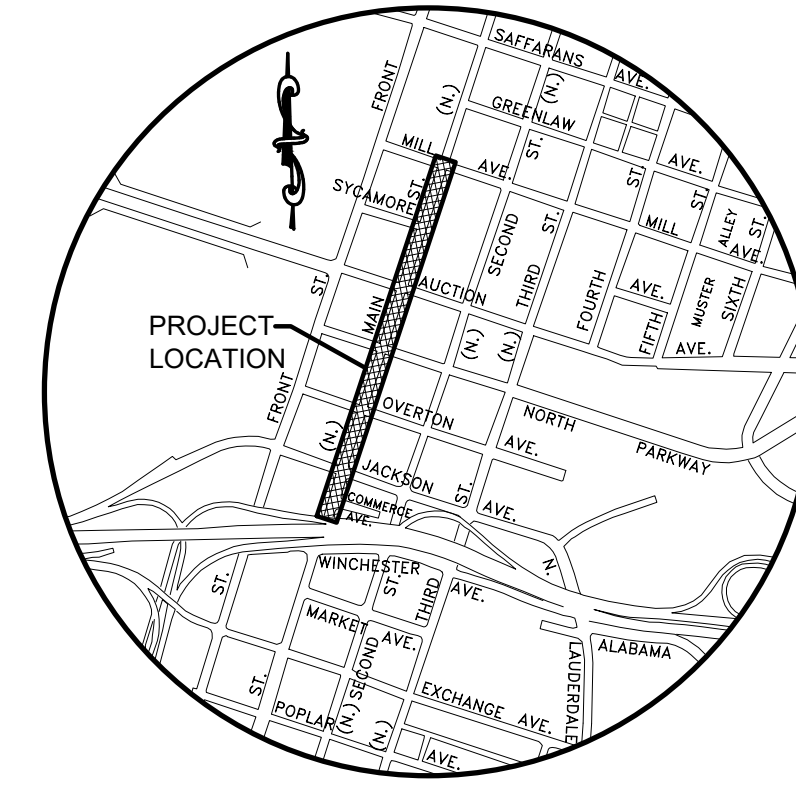
LOCATION: N MAIN STREET BETWEEN
 SHADYAC AVE. AND MILL AVE.
 MEMPHIS, TENNESSEE

SURVEY: N/A	DATE: N/A	DATE: N/A
DESIGN BY: SSR, INC.	DATE: APRIL 2022	SCALE: 1"=50'
REVIEWED:		

CITY TRAFFIC ENGINEER _____ DATE _____ CITY ENGINEER _____ DATE _____

SPECIAL NOTES:

1. THE CONTRACTOR SHALL NOT CLOSE MORE THAN ONE INTERSECTION ON N. MAIN ST. AT A TIME.
2. THE CONTRACTOR SHALL NOT IMPLEMENT A CLOSURE ON ANY INTERSECTION PRIOR TO RECEIVING APPROVAL FROM THE CITY OF MEMPHIS FOR EACH SPECIFIC APPLICATION.

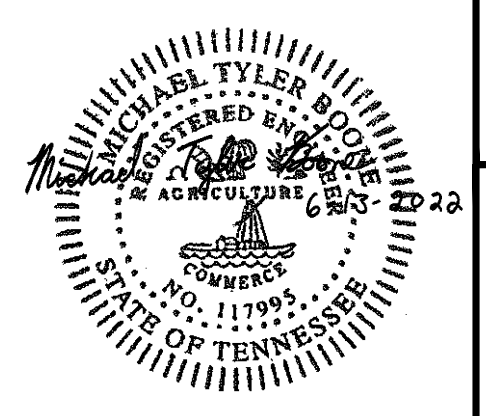


NOTES:

1. SEE SECTION 8F.03, SIGN PLACEMENT, OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR INFORMATION ON PLACEMENT AND MOUNTING OF SIGNS.
2. SIGNS SHOWN ON THIS PLAN ARE TO WARN TRAFFIC ABOUT THE CONSTRUCTION. OTHER TRAFFIC CONTROL DEVICES MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. NOTHING IN THIS PLAN IS INTENDED TO SUPERSEDE OR RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING THE APPROPRIATE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT STATE OF TENNESSEE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS."
4. CONTRACTOR SHALL BE REQUIRED TO NOTIFY THE CITY OF MEMPHIS CONSTRUCTION INSPECTION DEPARTMENT (636-2462) AND TRAFFIC ENGINEERING DEPARTMENT (636-6710) A MINIMUM OF 24 HOURS PRIOR TO COMMENCING CONSTRUCTION OR IMPLEMENTING A TRAFFIC CONTROL PLAN. ALL TRAFFIC CONTROL DEVICES MUST BE IN PLACE BEFORE CONSTRUCTION ACTIVITY BEGINS.
5. SIZES OF ALL SIGNS SHALL COMPLY WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
6. ALL TRAFFIC CONTROL DEVICES AND THEIR INSTALLATION SHALL MEET THE STANDARD PRESCRIBED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND SHALL COMPLY WITH THE STATE OF TENNESSEE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 712 TEMPORARY TRAFFIC CONTROL.
7. ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED WHENEVER POSSIBLE. THE CONTRACTOR IS RESPONSIBLE FOR COMPENSATING PROPERTY OWNERS THAT LOSE ACCESS DURING ROAD CLOSURE.
8. SIDE STREET, DRIVEWAY ACCESS, AND SAFE PEDESTRIAN WAYS SHALL BE MAINTAINED AT ALL TIMES.
9. THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THE RIGHT-OF-WAY OR WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHICHEVER IS LESS, WHEN THE LANE IS OPEN TO TRAFFIC, UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THE RIGHT-OF-WAY OR WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE WHICHEVER IS LESS, AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE.
10. CONTRACTOR SHALL USE PLASTIC DRUMS WITH TYPE "A" WARNING LIGHTS TO SEPARATE TRAFFIC FROM THE CONSTRUCTION AREA.
11. CONTRACTOR SHALL COVER ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SIGNS OR DEVICES DURING CONSTRUCTION AND THEY SHALL REMAIN COVERED DURING CONSTRUCTION AND UNTIL SUCH TIME THAT NO CONFLICT EXISTS.
12. CONTRACTOR SHALL INSTALL TEMPORARY NO PARKING SIGNS AS REQUIRED WITHIN THE TRAFFIC CONTROL AREA.
13. ALL TEMPORARY OR PERMANENT TRAVELED SURFACE SHALL BE INSPECTED DAILY BY THE CONTRACTOR (INCLUDING WEEKENDS) AND NECESSARY PATCHING OR RE-FINISHING PERFORMED.
14. MARKINGS SHALL BE MAINTAINED IN LONG-TERM STATIONARY WORK AREAS AND SHALL MATCH AND MEET THE MARKINGS IN PLACE AT BOTH ENDS OF THE WORK AREA.
15. CONTRACTOR SHALL CONTACT ALL APPROPRIATE AGENCIES BEFORE CLOSING ANY ROADWAYS.
16. CONTRACTOR SHALL CONTACT THE CITY OF MEMPHIS SIGNAL SHOP AT 528-2844 FOR LOCATION OF SIGNAL CONDUIT AND WIRES.
17. THE APPROPRIATE TRAFFIC CONTROL SHALL BE INSTALLED AT THE INCEPTION OF EACH STAGE OF CONSTRUCTION AND SHALL BE PROPERLY MAINTAINED AND/OR OPERATED DURING THE TIME SUCH SPECIAL CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS THEY ARE NEEDED AND SHALL BE IMMEDIATELY REMOVED THEREAFTER.
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19. ALL TRAFFIC CONTROL SIGNS SHALL MEET THE MINIMUM RETROREFLECTIVITY LEVELS SPECIFIED IN THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
20. THE CONTRACTOR IS PROHIBITED FROM CLOSING THE EXISTING SIDEWALK AT ANYTIME DURING THIS PROJECT. IF A SIDEWALK CLOSURE IS NEEDED, A REVISED TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO AND APPROVED BY THE CITY ENGINEER'S OFFICE.
21. ANY CLOSURE OF THE RIGHT OF WAY SHALL BE TIME LIMITED TO THE ACTIVE DEMOLITION OR CONSTRUCTION. CONTINUOUS UNWARRANTED CLOSURE OF THE RIGHT OF WAY SHALL NOT BE ALLOWED FOR THE DURATION OF THE PROJECT. THE DEVELOPER SHALL PROVIDE ON THE TRAFFIC CONTROL PLAN, THE TIME NEEDED PER PHASE TO COMPLETE THAT PORTION OF THE WORK. TIME LIMITS WILL BEGIN ON THE DAY OF CLOSURE AND WILL BE MONITORED BY THE ENGINEERING CONSTRUCTION INSPECTORS (636-2462) ON THE JOB.
22. THE CONTRACTOR SHALL NOTIFY MEMPHIS AREA TRANSIT AUTHORITY (MATA) AT 722-7100 A MINIMUM OF 48 HOURS PRIOR TO IMPLEMENTING A TRAFFIC CONTROL PLAN.
23. ONLY ONE PHASE OF THE TRAFFIC CONTROL PLAN SHALL BE ACTIVE AT ANY ONE TIME.

DESIGN SPEED LIMIT: 35 MPH
DATE: TBD
DURATION: UP TO 5 DAYS

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



TC1.1

SHEET 2 OF 4

TRAFFIC CONTROL PLAN FOR REPAIR AT INTERSECTION OF N. MAIN ST. & A.W. WILLIS AVE.

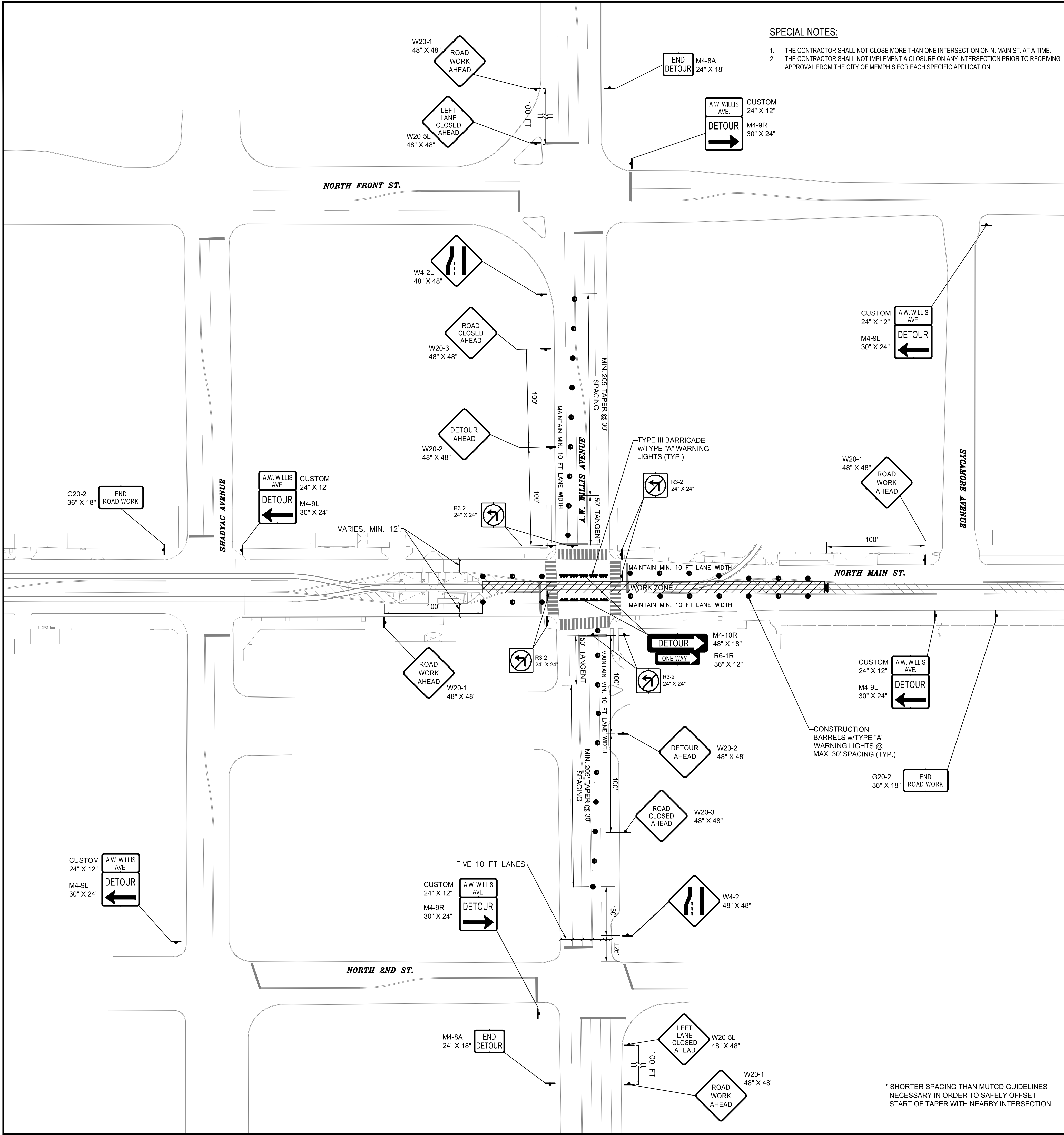
LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.

DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

SURVEY: N/A
DESIGN BY: SSR, INC. DATE: APRIL 2022
REVIEWED: DATE: N/A SCALE: 1"=50'

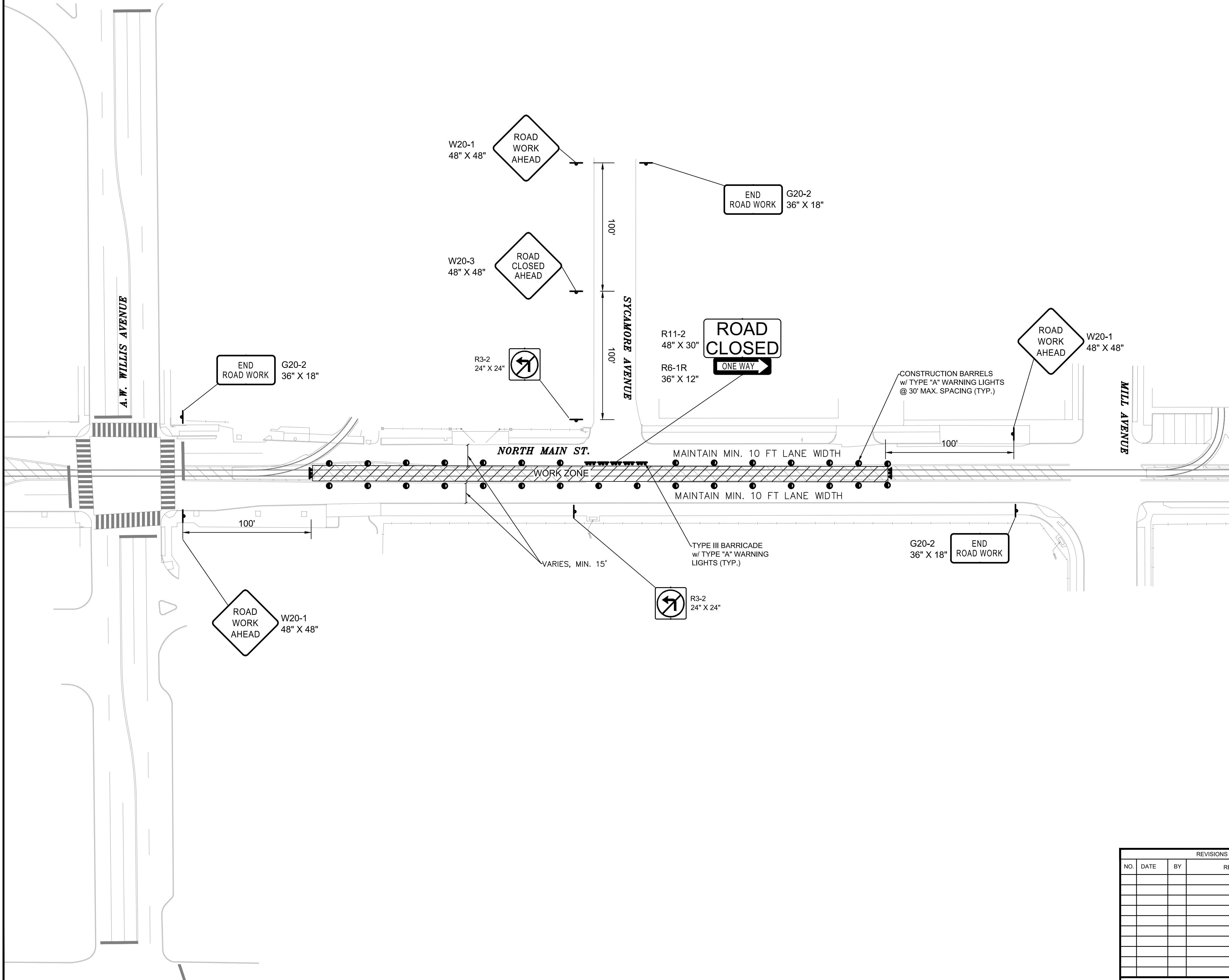
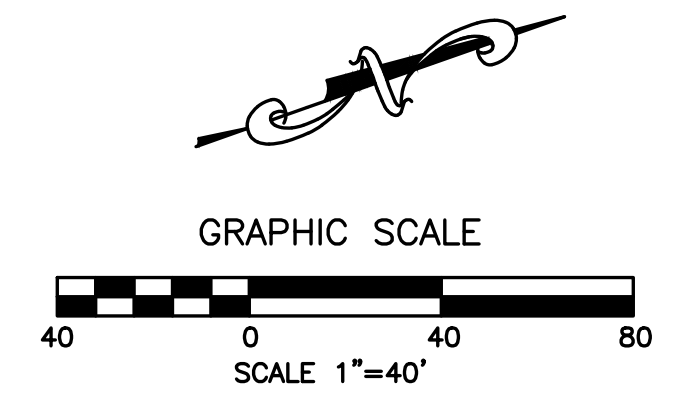
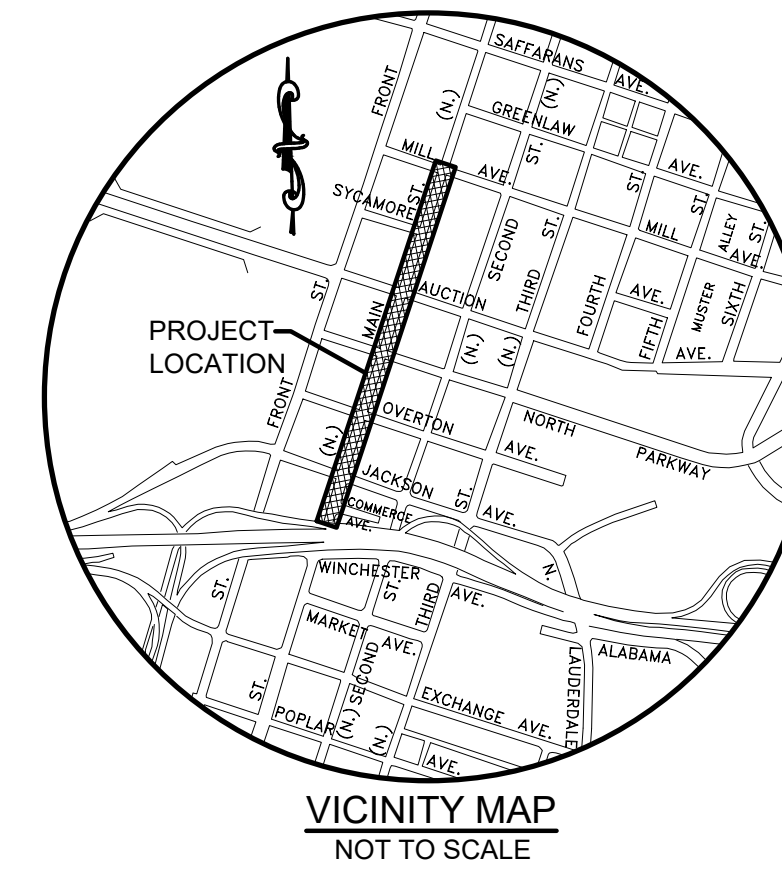
CITY TRAFFIC ENGINEER: _____ DATE: _____ CITY ENGINEER: _____ DATE: _____



* SHORTER SPACING THAN MUTCD GUIDELINES NECESSARY IN ORDER TO SAFELY OFFSET START OF TAPER WITH NEARBY INTERSECTION.

SPECIAL NOTES:

1. THE CONTRACTOR SHALL NOT CLOSE MORE THAN ONE INTERSECTION ON N. MAIN ST. AT A TIME.
2. THE CONTRACTOR SHALL NOT IMPLEMENT A CLOSURE ON ANY INTERSECTION PRIOR TO RECEIVING APPROVAL FROM THE CITY OF MEMPHIS FOR EACH SPECIFIC APPLICATION.

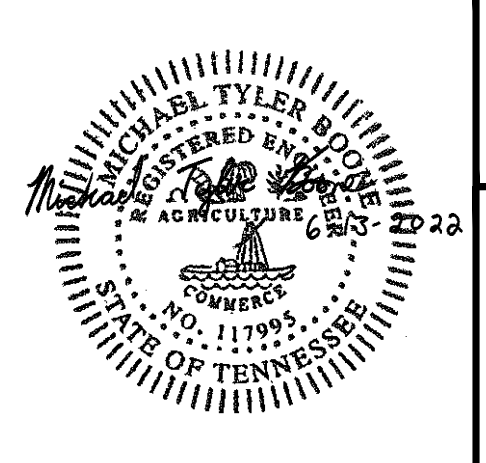


NOTES:

1. SEE SECTION 8F.03, SIGN PLACEMENT, OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR INFORMATION ON PLACEMENT AND MOUNTING OF SIGNS.
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14. MARKINGS SHALL BE MAINTAINED IN LONG-TERM STATIONARY WORK AREAS AND SHALL MATCH AND MEET THE MARKINGS IN PLACE AT BOTH ENDS OF THE WORK AREA.
15. CONTRACTOR SHALL CONTACT ALL APPROPRIATE AGENCIES BEFORE CLOSING ANY ROADWAYS.
16. CONTRACTOR SHALL CONTACT THE CITY OF MEMPHIS SIGNAL SHOP AT 528-2844 FOR LOCATION OF SIGNAL CONDUIT AND WIRES.
17. THE APPROPRIATE TRAFFIC CONTROL SHALL BE INSTALLED AT THE INCEPTION OF EACH STAGE OF CONSTRUCTION AND SHALL BE PROPERLY MAINTAINED AND/OR OPERATED DURING THE TIME SUCH SPECIAL CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS THEY ARE NEEDED AND SHALL BE IMMEDIATELY REMOVED THEREAFTER.
18. EXISTING STRIPING THAT CONFLICTS WITH THE TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE COVERED OR REMOVED DURING CONSTRUCTION. WHEN CONSTRUCTION IS COMPLETE THE EXISTING STRIPING SHALL BE RETURNED TO ITS ORIGINAL STATE.
19. ALL TRAFFIC CONTROL SIGNS SHALL MEET THE MINIMUM RETROREFLECTIVITY LEVELS SPECIFIED IN THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
20. THE CONTRACTOR IS PROHIBITED FROM CLOSING THE EXISTING SIDEWALK AT ANYTIME DURING THIS PROJECT. IF A SIDEWALK CLOSURE IS NEEDED, A REVISED TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO AND APPROVED BY THE CITY ENGINEER'S OFFICE.
21. ANY CLOSURE OF THE RIGHT OF WAY SHALL BE TIME LIMITED TO THE ACTIVE DEMOLITION OR CONSTRUCTION. CONTINUOUS UNWARRANTED CLOSURE OF THE RIGHT OF WAY SHALL NOT BE ALLOWED FOR THE DURATION OF THE PROJECT. THE DEVELOPER SHALL PROVIDE ON THE TRAFFIC CONTROL PLAN, THE TIME NEEDED PER PHASE TO COMPLETE THAT PORTION OF THE WORK. TIME LIMITS WILL BEGIN ON THE DAY OF CLOSURE AND WILL BE MONITORED BY THE ENGINEERING CONSTRUCTION INSPECTORS (636-2462) ON THE JOB.
22. THE CONTRACTOR SHALL NOTIFY MEMPHIS AREA TRANSIT AUTHORITY (MATA) AT 722-7100 A MINIMUM OF 48 HOURS PRIOR TO IMPLEMENTING A TRAFFIC CONTROL PLAN.
23. ONLY ONE PHASE OF THE TRAFFIC CONTROL PLAN SHALL BE ACTIVE AT ANY ONE TIME.

DESIGN SPEED LIMIT: 35 MPH
 DATE: TBD
 DURATION: UP TO 5 DAYS

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



SHEET 3 OF 4
TC1.2
TRAFFIC CONTROL PLAN FOR REPAIR OF SINGLE TRACK BETWEEN LANES
 LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE. MEMPHIS, TENNESSEE

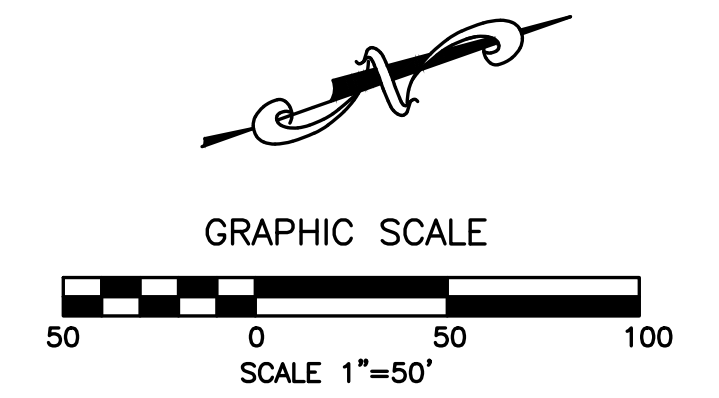
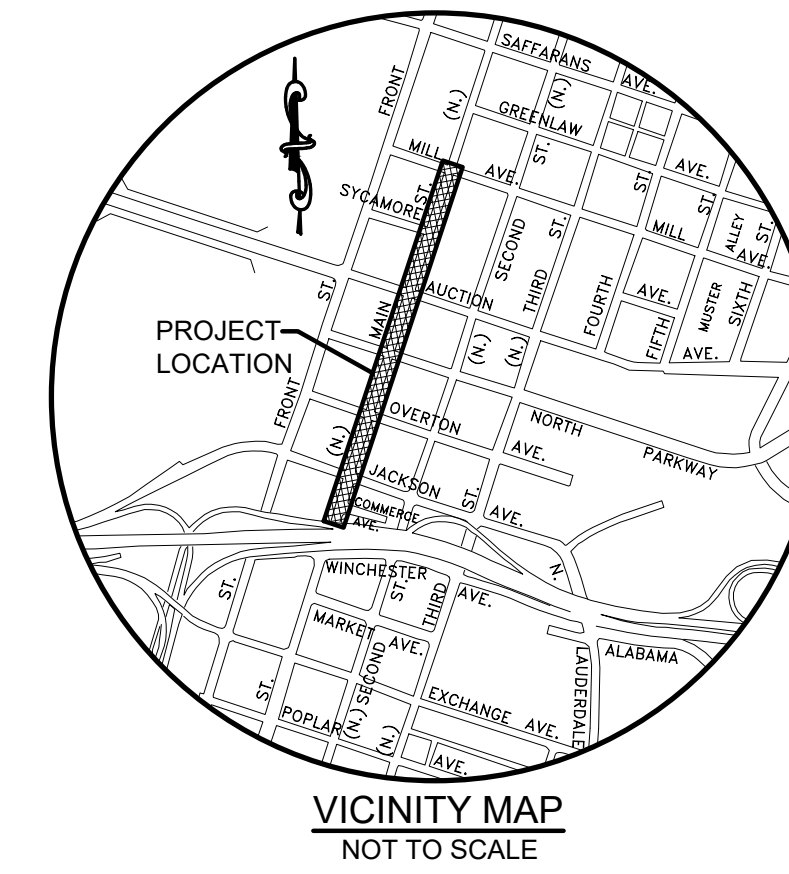
TRACK SLAB PAVEMENT REPAIR
 NORTH MAIN ST.
 DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
 ENGINEER: SSR, INC.

SURVEY: N/A
 DESIGN BY: SSR, INC.
 DATE: APRIL 2022
 REVIEWED: _____

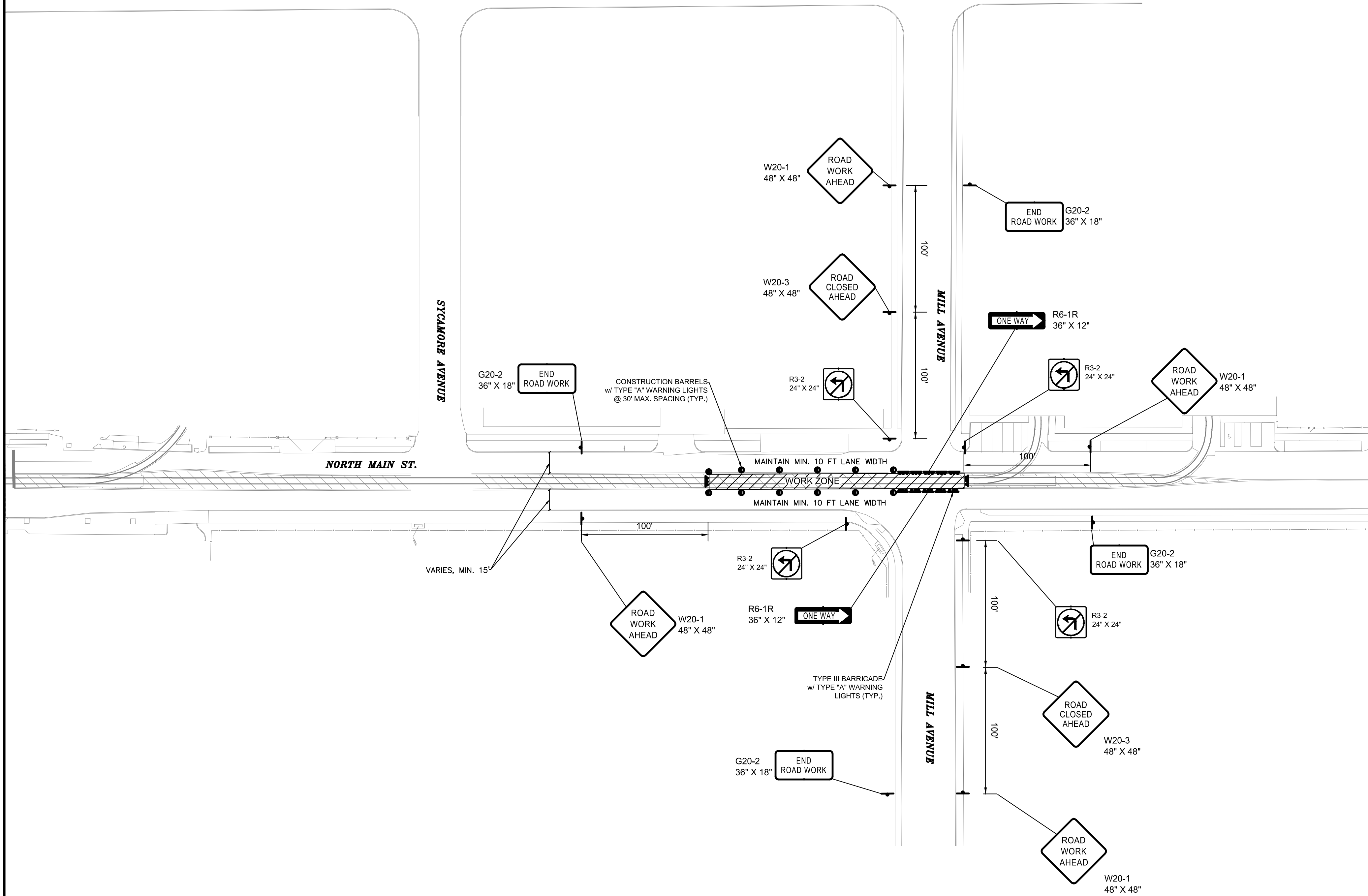
DATE: N/A
 SCALE: 1"=40'
 DATE: _____
 CITY TRAFFIC ENGINEER: _____
 CITY ENGINEER: _____
 DATE: _____

SPECIAL NOTES:

1. THE CONTRACTOR SHALL NOT CLOSE MORE THAN ONE INTERSECTION ON N. MAIN ST. AT A TIME.
2. THE CONTRACTOR SHALL NOT IMPLEMENT A CLOSURE ON ANY INTERSECTION PRIOR TO RECEIVING APPROVAL FROM THE CITY OF MEMPHIS FOR EACH SPECIFIC APPLICATION.



NORTH FRONT ST.

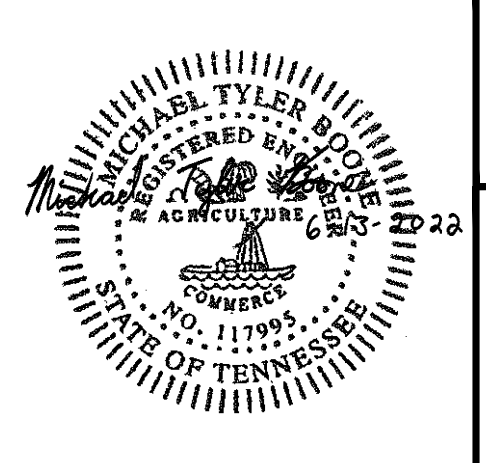


NOTES:

1. SEE SECTION 8F.03, SIGN PLACEMENT, OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR INFORMATION ON PLACEMENT AND MOUNTING OF SIGNS.
2. SIGNS SHOWN ON THIS PLAN ARE TO WARN TRAFFIC ABOUT THE CONSTRUCTION. OTHER TRAFFIC CONTROL DEVICES MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. NOTHING IN THIS PLAN IS INTENDED TO SUPERSEDE OR RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING THE APPROPRIATE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT STATE OF TENNESSEE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS."
4. CONTRACTOR SHALL BE REQUIRED TO NOTIFY THE CITY OF MEMPHIS CONSTRUCTION INSPECTION DEPARTMENT (636-2462) AND TRAFFIC ENGINEERING DEPARTMENT (636-6710) A MINIMUM OF 24 HOURS PRIOR TO COMMENCING CONSTRUCTION OR IMPLEMENTING A TRAFFIC CONTROL PLAN. ALL TRAFFIC CONTROL DEVICES MUST BE IN PLACE BEFORE CONSTRUCTION ACTIVITY BEGINS.
5. SIZES OF ALL SIGNS SHALL COMPLY WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
6. ALL TRAFFIC CONTROL DEVICES AND THEIR INSTALLATION SHALL MEET THE STANDARD PRESCRIBED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND SHALL COMPLY WITH THE STATE OF TENNESSEE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 712 TEMPORARY TRAFFIC CONTROL.
7. ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED WHENEVER POSSIBLE. THE CONTRACTOR IS RESPONSIBLE FOR COMPENSATING PROPERTY OWNERS THAT LOSE ACCESS DURING ROAD CLOSURE.
8. SIDE STREET, DRIVEWAY ACCESS, AND SAFE PEDESTRIAN WAYS SHALL BE MAINTAINED AT ALL TIMES.
9. THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THE RIGHT-OF-WAY OR WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHICHEVER IS LESS, WHEN THE LANE IS OPEN TO TRAFFIC, UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THE RIGHT-OF-WAY OR WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE WHICHEVER IS LESS, AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE.
10. CONTRACTOR SHALL USE PLASTIC DRUMS WITH TYPE "A" WARNING LIGHTS TO SEPARATE TRAFFIC FROM THE CONSTRUCTION AREA.
11. CONTRACTOR SHALL COVER ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SIGNS OR DEVICES DURING CONSTRUCTION AND THEY SHALL REMAIN COVERED DURING CONSTRUCTION AND UNTIL SUCH TIME THAT NO CONFLICT EXISTS.
12. CONTRACTOR SHALL INSTALL TEMPORARY NO PARKING SIGNS AS REQUIRED WITHIN THE TRAFFIC CONTROL AREA.
13. ALL TEMPORARY OR PERMANENT TRAVELED SURFACE SHALL BE INSPECTED DAILY BY THE CONTRACTOR (INCLUDING WEEKENDS) AND NECESSARY PATCHING OR RE-FINISHING PERFORMED.
14. MARKINGS SHALL BE MAINTAINED IN LONG-TERM STATIONARY WORK AREAS AND SHALL MATCH AND MEET THE MARKINGS IN PLACE AT BOTH ENDS OF THE WORK AREA.
15. CONTRACTOR SHALL CONTACT ALL APPROPRIATE AGENCIES BEFORE CLOSING ANY ROADWAYS.
16. CONTRACTOR SHALL CONTACT THE CITY OF MEMPHIS SIGNAL SHOP AT 528-2844 FOR LOCATION OF SIGNAL CONDUIT AND WIRES.
17. THE APPROPRIATE TRAFFIC CONTROL SHALL BE INSTALLED AT THE INCEPTION OF EACH STAGE OF CONSTRUCTION AND SHALL BE PROPERLY MAINTAINED AND/OR OPERATED DURING THE TIME SUCH SPECIAL CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS THEY ARE NEEDED AND SHALL BE IMMEDIATELY REMOVED THEREAFTER.
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22. THE CONTRACTOR SHALL NOTIFY MEMPHIS AREA TRANSIT AUTHORITY (MATA) AT 722-7100 A MINIMUM OF 48 HOURS PRIOR TO IMPLEMENTING A TRAFFIC CONTROL PLAN.
23. ONLY ONE PHASE OF THE TRAFFIC CONTROL PLAN SHALL BE ACTIVE AT ANY ONE TIME.

DESIGN SPEED LIMIT: 35 MPH
DATE: TBD
DURATION: UP TO 5 DAYS

REVISIONS				APPROVED	
NO.	DATE	BY	REMARKS	C.T.E.	C.E.



TRACK SLAB PAVEMENT REPAIR
NORTH MAIN ST.
DEVELOPER: MEMPHIS AREA TRANSIT AUTHORITY
ENGINEER: SSR, INC.

TC1.3

SHEET 4 OF 4

TRAFFIC CONTROL PLAN FOR REPAIR OF SINGLE TRACK AT INTERSECTION OF N MAIN ST. AND MILL AVE.
LOCATION: N MAIN STREET BETWEEN SHADYAC AVE. AND MILL AVE.
MEMPHIS, TENNESSEE

SURVEY	N/A	DATE	APRIL 2022	DATE	N/A
DESIGN BY	SSR, INC.	SCALE	1"=50'	REVIEWED:	
CITY TRAFFIC ENGINEER		DATE		CITY ENGINEER	